

THE AT.&S.F. RY. SYSTEM

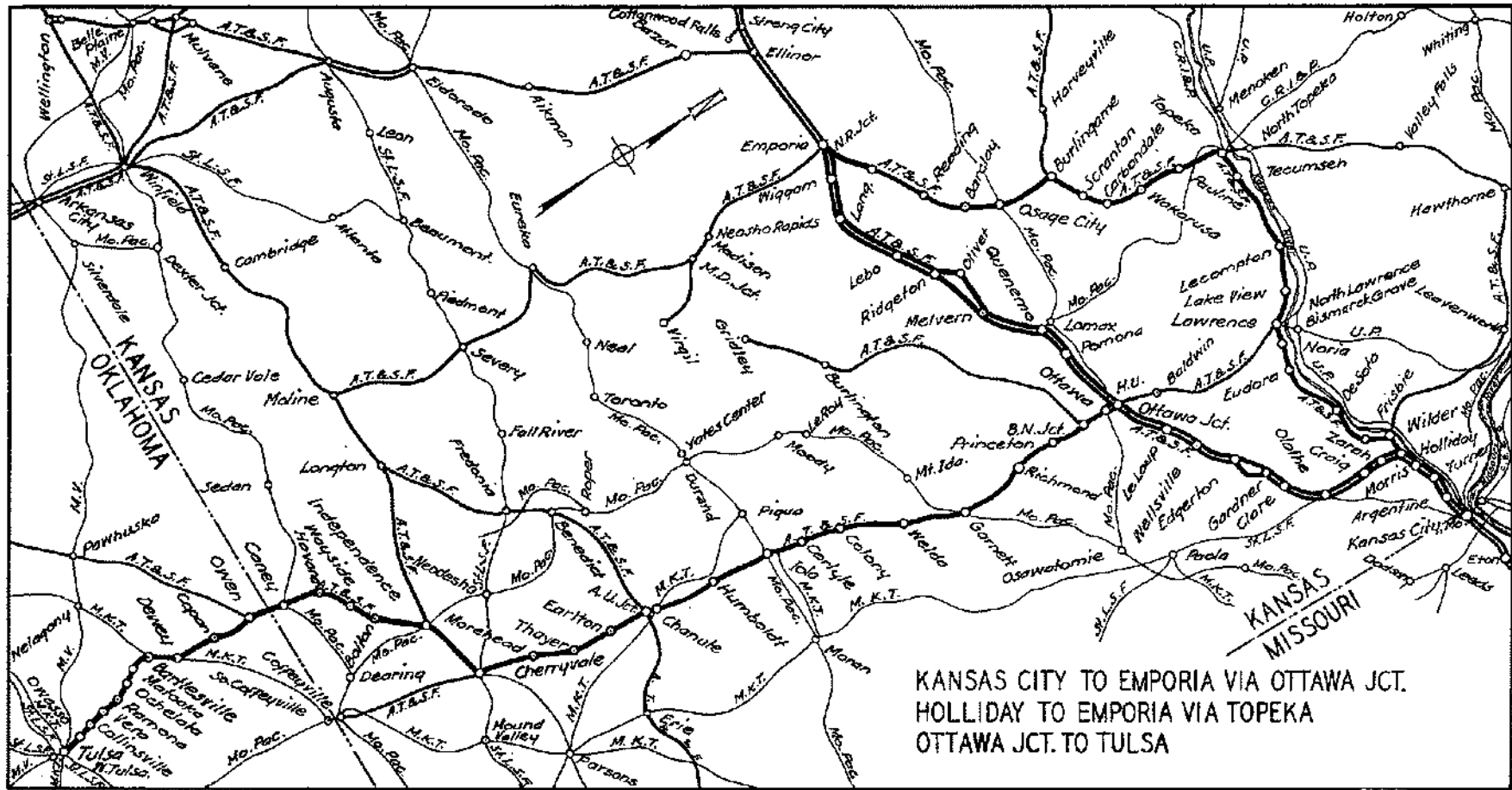
TRACK CHART

KANSAS CITY TO EMPORIA VIA OTTAWA JCT.
HOLLIDAY TO EMPORIA VIA TOPEKA
OTTAWA JCT. TO TULSA

Corrected to July, 1946
JAN. 1960

C.E.O.T. 18589
18590

Changed 1/8/63 mdr



KANSAS CITY TO EMPORIA VIA OTTAWA JCT.
 HOLLIDAY TO EMPORIA VIA TOPEKA
 OTTAWA JCT. TO TULSA

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Argentine-Turner Yard		2 & 2-A.

EXPLANATIONS

Miles Alignment

436 ^{#190}
1°00'
I=23°35' 435

Curve to the left indicated
El. 2 1/2"
Sp. 300'

Grade Line

0.45 0.50 0.40 0.30 0.0

725.3
721.9
718.6
714.6

Sig. 4353
Br. 4357, O.P. Co. Hwy.
164 Truss on C.

Missouri R.R.

Track Diagram

X W.B.
E.B. X

F.I.L.U.B.
sig. 4352

Br 435, 1, 36' x 96' C.I.P.


Scales — 1" = 3000' { Horizontal scale for profile,
alignment and tracks.
1" = 200' Vertical Scale for profile.

Ascending ruling grades for District.

Figures on grade lines are % gradients.
Figures below grade lines are profile sub-grade elevations in feet.

x = All types of Signals, except Bridge Signals.

• = Columns (Water & Oil)

 Permanent slow speed sign with appropriate speeds shown.

Car capacity — based on 50 ft. overall allowance per car.

BRIDGES

Ab. Abutment
 Ap. Approach
 Ar. Arch
 B.D. Ballast Deck
 Brk. Brick
 Br. Bridge
 Bx. Box
 C. Concrete
 C.I.P. Cast Iron Pipe
 C.P. Concrete Pipe
 Cor. P. Corrugated Pipe
 D.G. Deck Girder
 D.T. Deck Truss
 Dbl. Double
 Dr. Sp. Draw Span
 Ext. Extension
 Ftgs. Footings
 F.T. Frame Trestle
 Hdrl. Handrail

FACILITIES

Cdr. Cv. Cinder Conveyor
 Coa. Ch. Coal Chute
 F. Fuel
 O. Col. Oil Column
 O. T. Oil Tank
 Stk. Yd. Stock Yard
 T.T. Treating Tank
 T. Turntable
 W. Water
 W. Col. Water Column
 W. T. Water Tank
 Y. Wye

ABBREVIATIONS

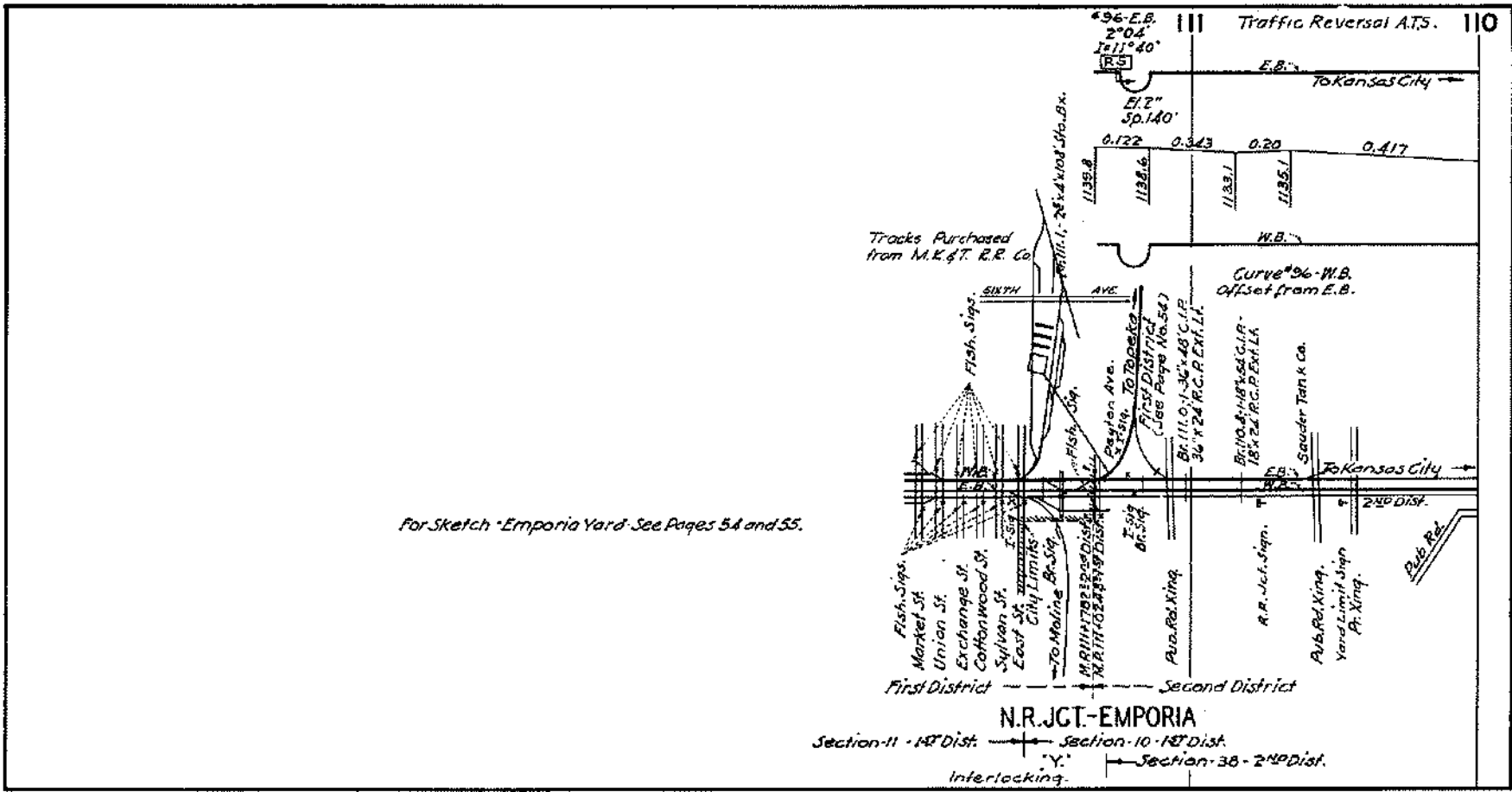
I. Bm. I. Beam
 Lg. Long
 Msy. Masonry
 O.D. Open Deck
 Peds. Pedestals
 P.T. Pile Trestle
 R.C. Reinforced Concrete
 S.S.C. Steel Stringers & Caps
 Sto. Stone
 T.R. T-Rail
 Th. G. Thru Girder
 Th. T. Thru Truss
 T. Bx. Timber Box
 Tr. Treated
 Trip. Triple
 Vit. P. Vitrified Pipe
 Wr. I. P. Wrol. Iron Pipe

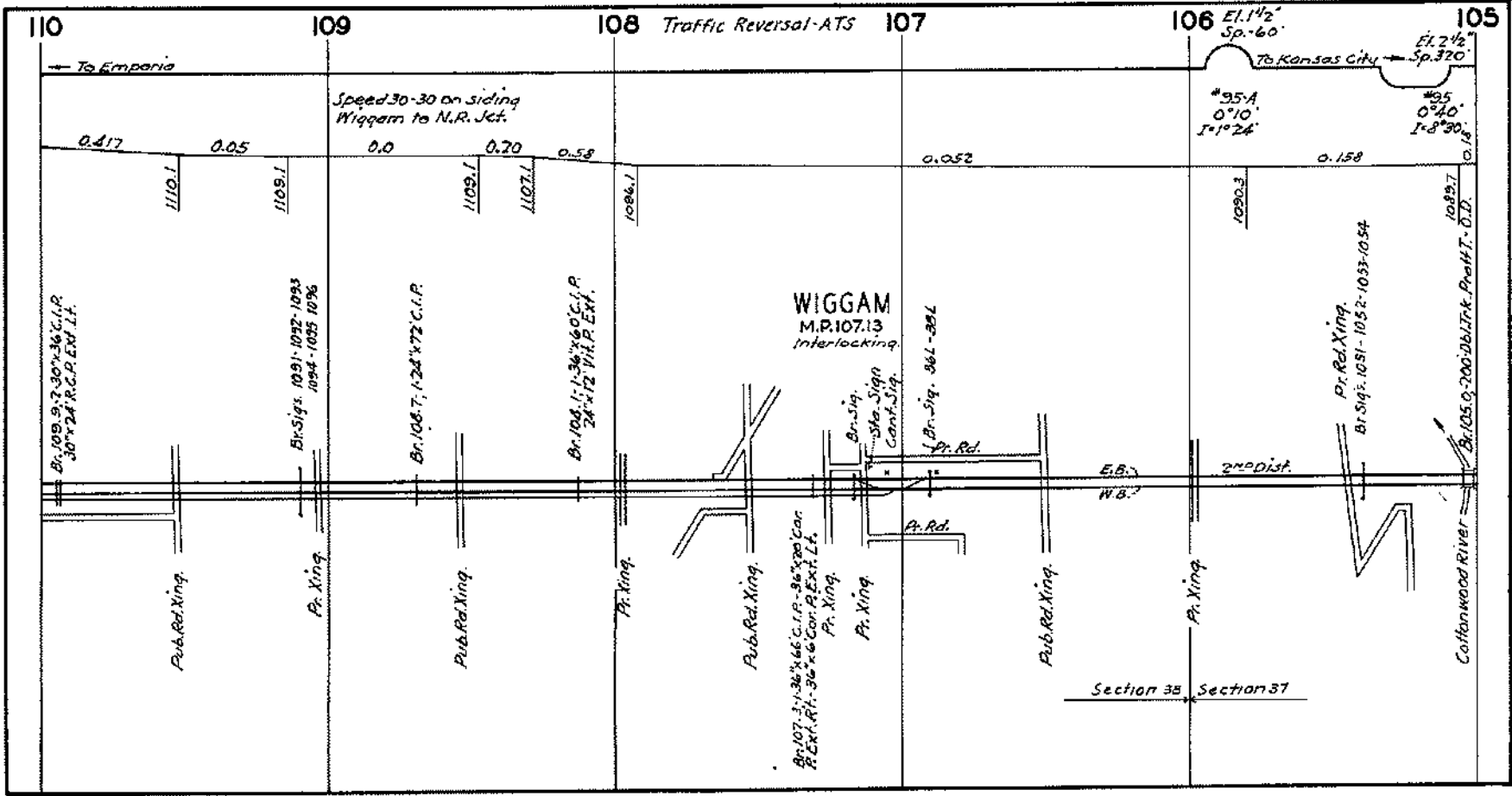
MISCELLANEOUS

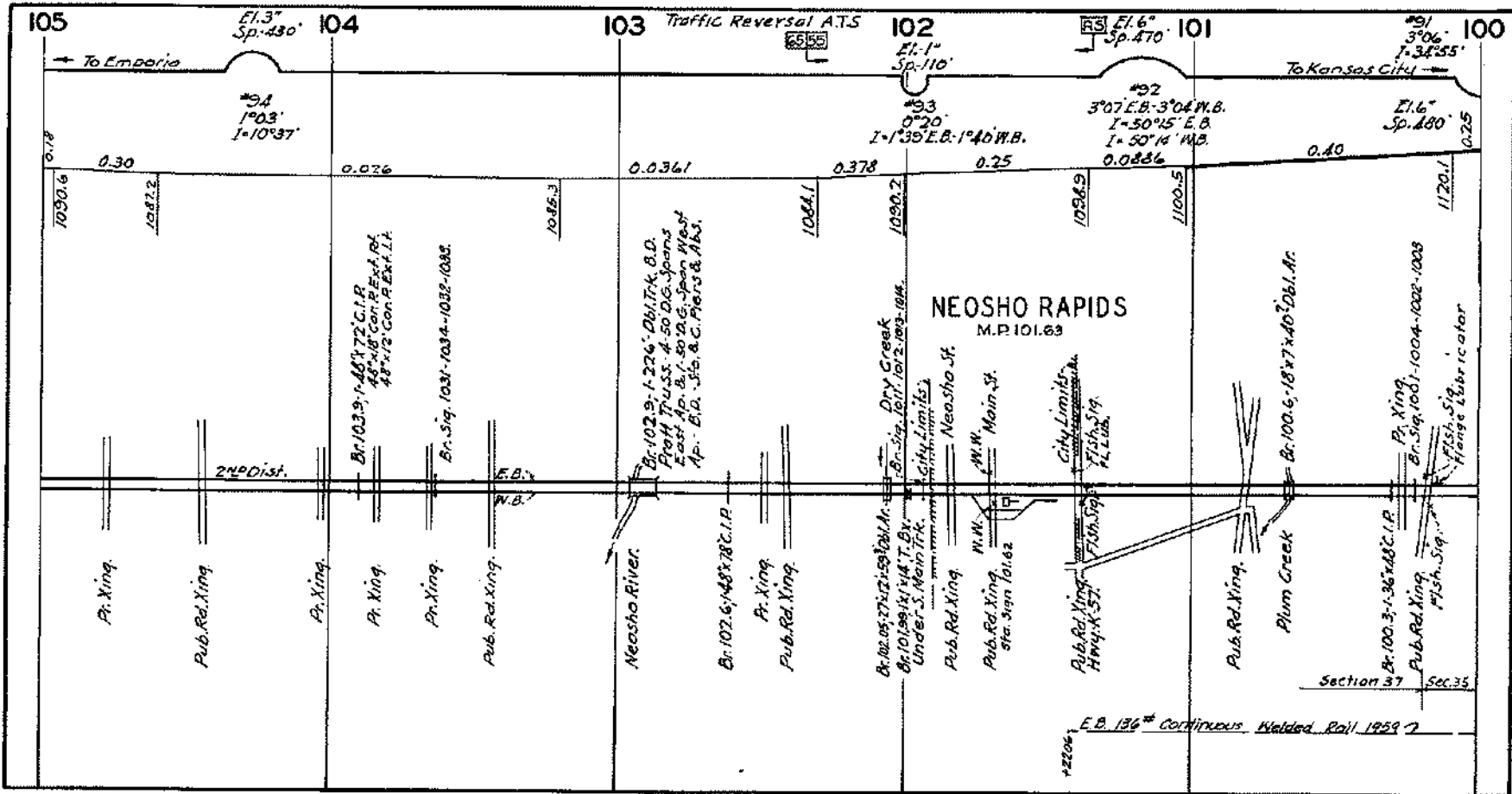
A.T.S. Automatic Train Stop
 Auto. Gates Automatic Gates
 Ave. Avenue
 Bch. Branch
 Blk. Block
 Br. Sig. Bridge Signal
 B.P. Bumping Post
 Cant. Sig. Cantilever Signal
 Cr. Creek
 Co. County
 Div. Division
 E.B. East Bound
 El. Elevation of Curves
 Exp. Bldg. Express Building
 Fish. Sig. Flashing Signal
 F.L.T.P. Flood Light Pole
 F.L.T. Flood Light Tower
 Frt. Freight
 Gr. At Grade
 Ho. House
 Hwy. Highway
 I. Total Curve Angle
 I. Sig. Interlocking Signal
 I. Sw. Interlocking Switch
 I. Tower Interlocking Tower
 Jct. Junction
 Lav. Lavatory
 Lt. Left
 N.B. North Bound
 O.P. Overpass
 Psgr. Passenger
 Pass. Passing
 Po. Power
 Pr. Private
 Pub. Public
 R.R. Railroad
 Ry. Railway
 Reserv. Reservoir
 R.S. Resume Speed
 Rt. Right
 R.W. Right of Way
 Riv. River
 Rd. Xing. Road Crossing
 Sec. Section
 Sig. Signal
 S.B. South Bound
 Sp. Spiral
 S. Sw. Spring Switch
 St. State, also Street
 Sta. Station
 Trk. Track
 U.P. Underpass
 W.B. West Bound
 W.W. Wig Wag Signal
 Yd. Yard
 Hw. Def. High Water Detector
 Ft. Lub. Flange Lubricator

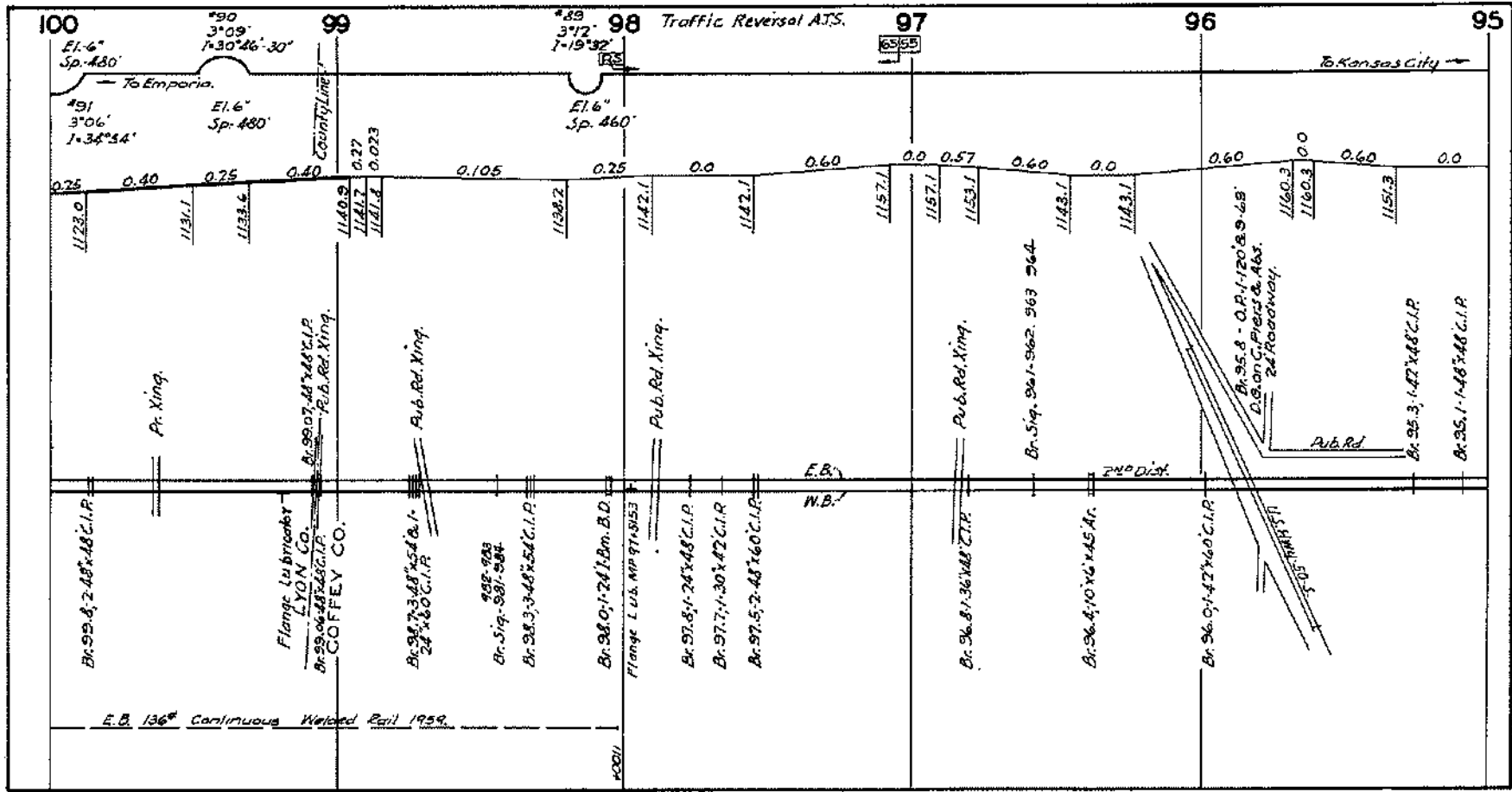
Add "s" to the above abbreviations when plural form required.

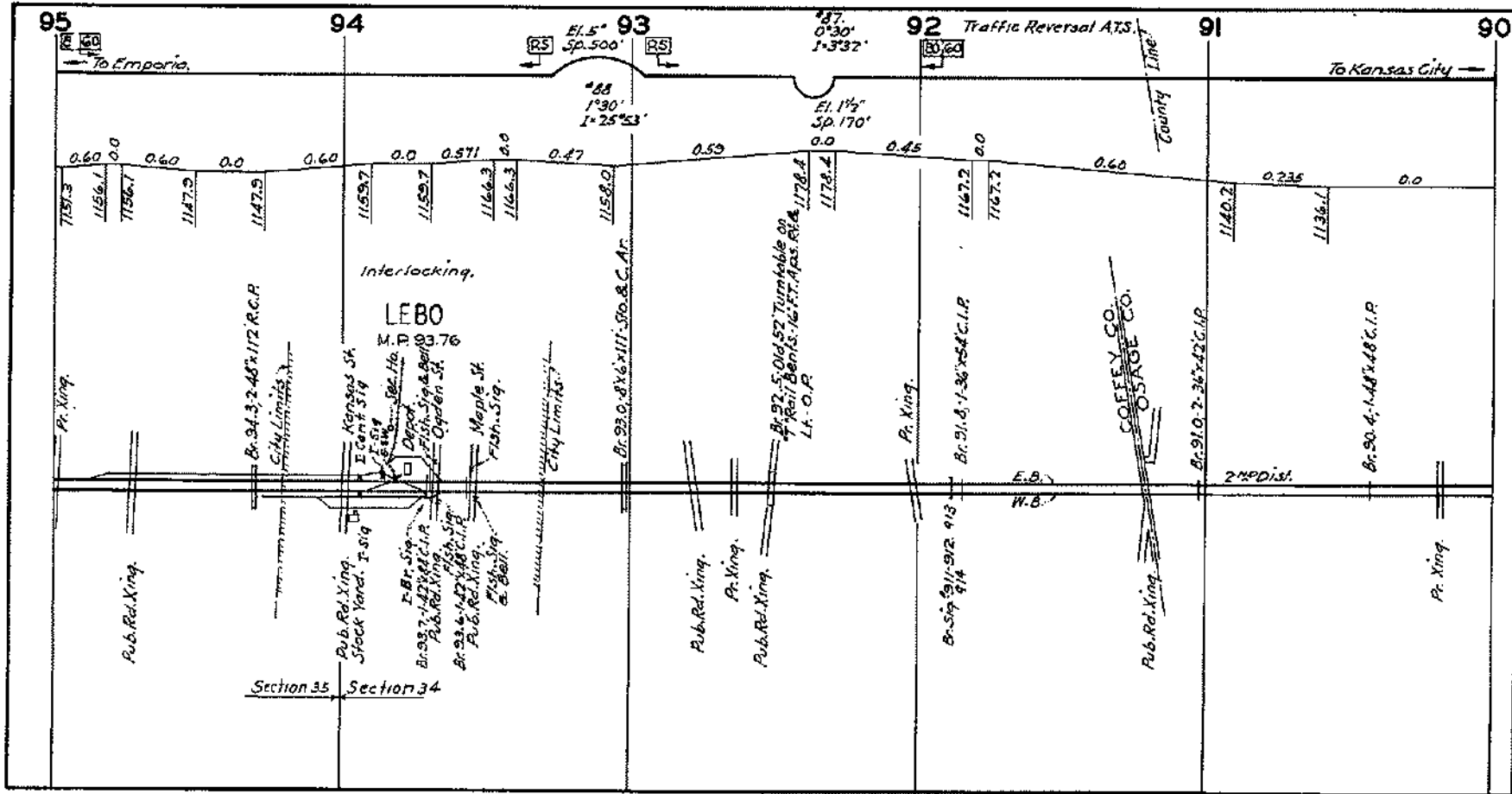
For Sketch "Emporia Yard" See Pages 54 and 55.







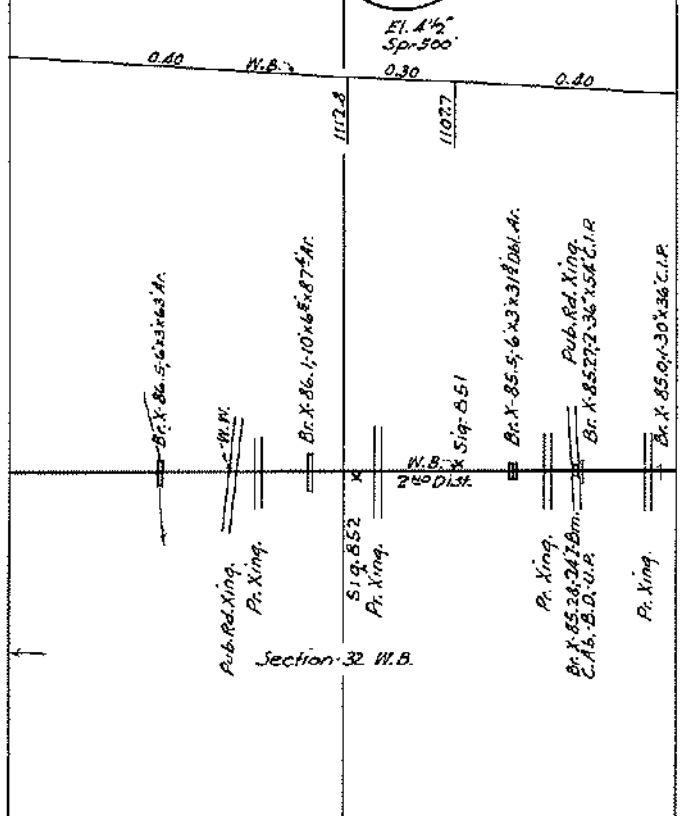




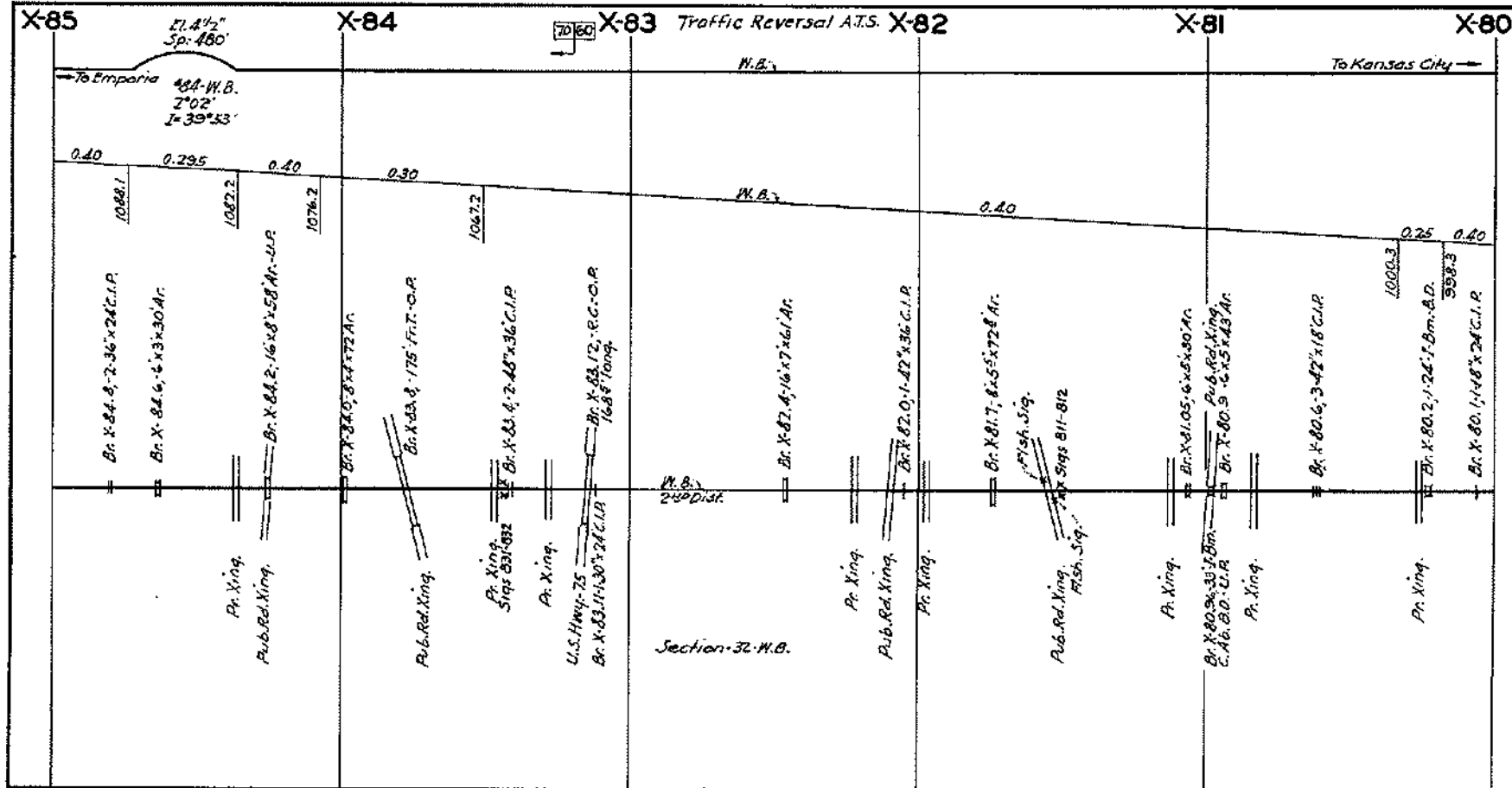
X-87 Traffic Reversal ATS X-86 85 W.B. 2°04' I=34°30' X-85

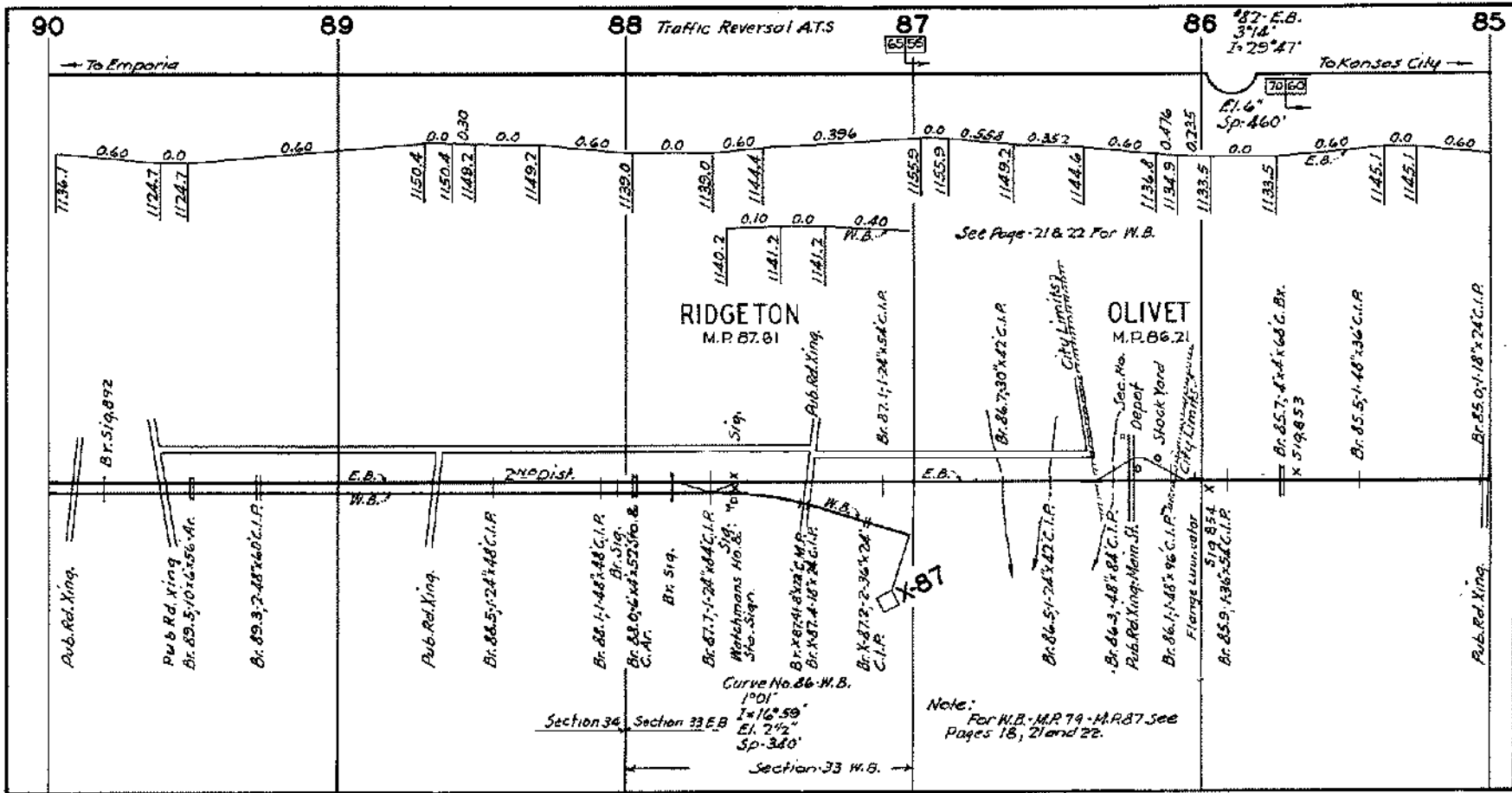
To Emporia W.B.

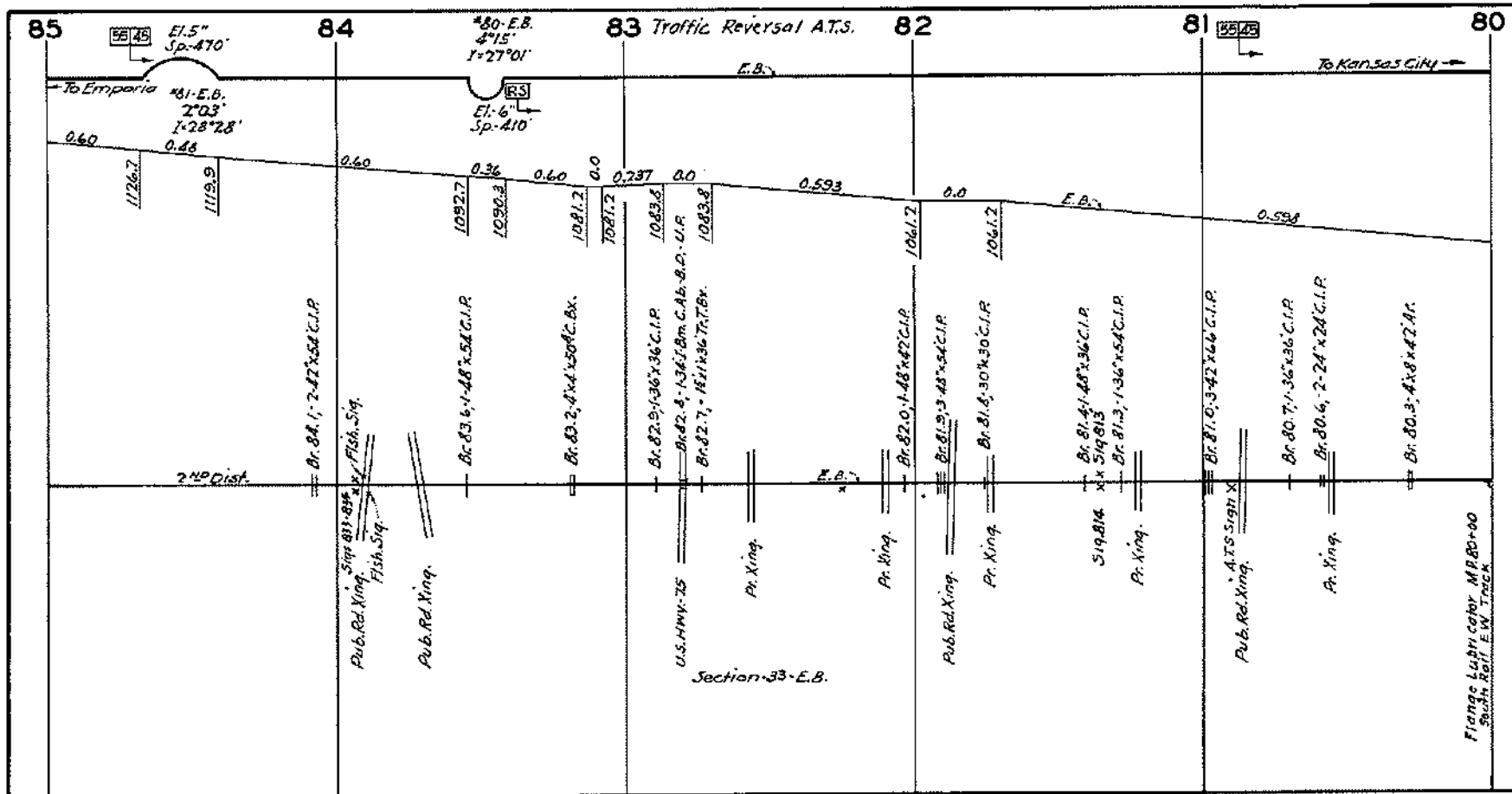
To Kansas City

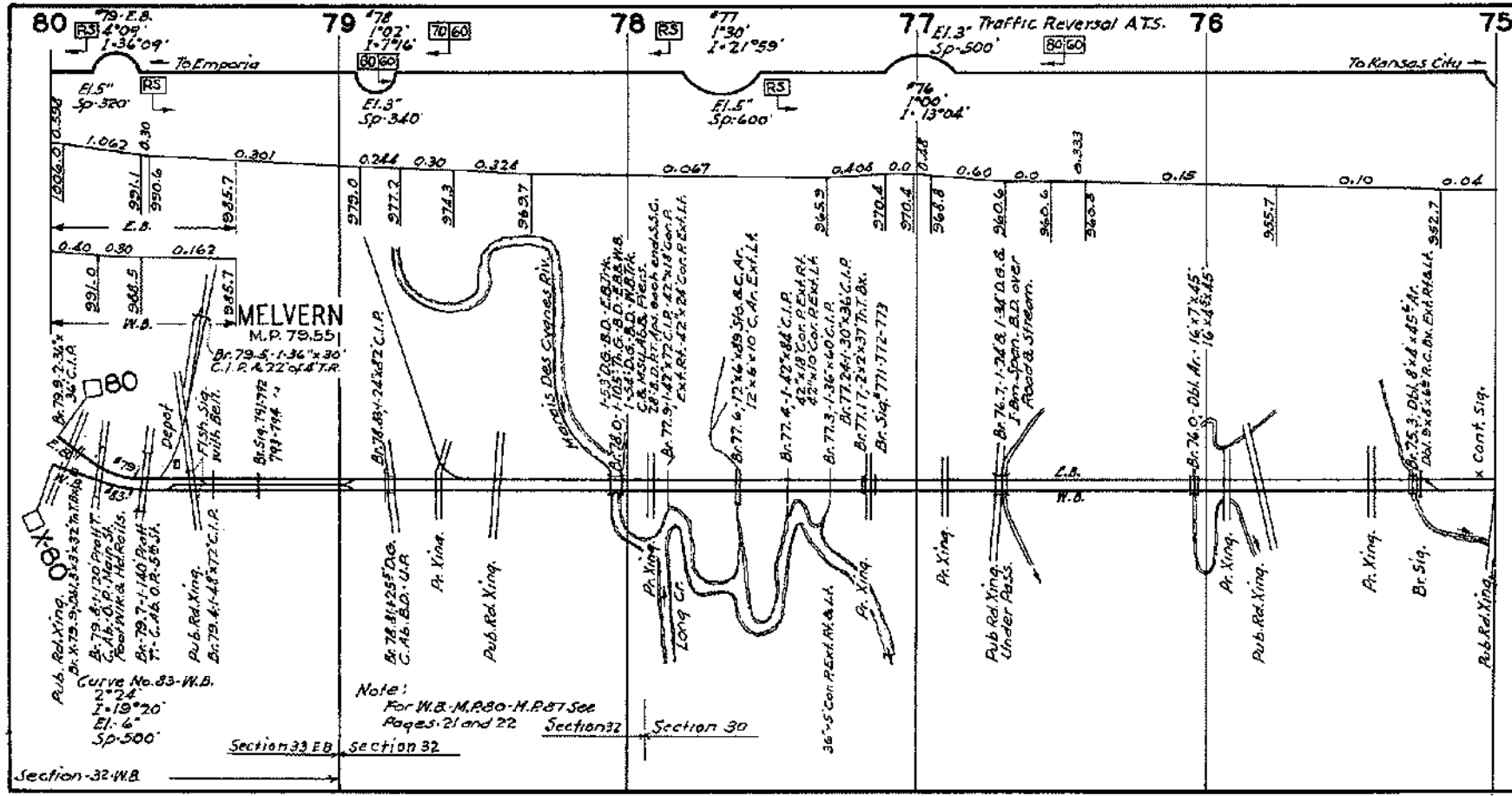


Continued on p. 20









Section-32-WB

Section 33 EB

Section 32

Section 32

Section 30

Note:
 For W.B. M.P. 80-H.P. 87 See
 Pages 21 and 22 Section 32

36'-5" CON. PENT. H.A.L.A.

Pub. Red. Xing.
 Br. X 79.5-304.3-32 in 1888
 C. Ab. O.P. Main St.
 Foot Walk a. Hd. Rails.
 Br. 79.7-140 Prof.
 7'-C. Ab. O.P. 35 St.
 Pub. Red. Xing.
 Br. 79.4-148 x 72' C.I.P.
 Fish Sig.
 with Bell
 Br. 519.7-1972
 791-794
 Br. 79.5-136 x 30'
 C.I.R. & 72' of $\Delta T.R.$

Curve No. 83-W.B.
 $R=136'$
 $\Delta=30'$
 $C.I.R.=72'$ of $\Delta T.R.$
 $E1=6'$
 $Sp=500'$

Pub. Red. Xing.
 C.A.S. B.D. - U.R.
 Br. 78.8-125' D.O.
 Br. 78.8-125' D.O.
 C.A.S. B.D. - U.R.

Pr. Xing.
 Pub. Red. Xing.
 Br. 78.4-125' D.O.
 C.A.S. B.D. - U.R.

Pr. Xing.
 Long Cr.
 Br. 77.9-142 x 72' C.I.P.
 42' x 18' Con. PENT. H.A.L.A.
 42' x 10' Con. PENT. H.A.L.A.
 42' x 10' Con. PENT. H.A.L.A.
 Br. 77.3-136' x 40' C.I.P.
 Br. 77.2-141-30' x 36' C.I.P.
 Br. 77.1-72 x 37' Tr. Ab.
 Br. Sig. 771-772-773

Pr. Xing.
 Br. 77.6-12 x 6' 189.56' C.C. Ar.
 12' x 6' 10' C. Ar. Exh. Lt.

Pr. Xing.
 Br. 77.4-142 x 84' C.I.P.
 42' x 18' Con. PENT. H.A.L.A.
 42' x 10' Con. PENT. H.A.L.A.
 Br. 77.3-136' x 40' C.I.P.
 Br. 77.2-141-30' x 36' C.I.P.
 Br. 77.1-72 x 37' Tr. Ab.
 Br. Sig. 771-772-773

Pr. Xing.
 Pub. Red. Xing.
 Under Pass.
 Br. 76.7-124' & 134' D.O. &
 19m. Span. B.D. over
 Abard & Stream.

Pr. Xing.
 Pub. Red. Xing.
 Br. 76.0-161' Ar.
 16' x 7' x 45'
 16' x 8' x 45'

Pr. Xing.
 Pub. Red. Xing.
 Br. 75.3-161' & 145' Ar.
 16' x 8' x 45' R.C. Br. Exh. H.A.L.A.
 952.7

Pr. Xing.
 Pub. Red. Xing.
 Br. 75.3-161' & 145' Ar.
 16' x 8' x 45' R.C. Br. Exh. H.A.L.A.
 952.7

Pr. Xing.
 Pub. Red. Xing.
 Br. 75.3-161' & 145' Ar.
 16' x 8' x 45' R.C. Br. Exh. H.A.L.A.
 952.7

Pr. Xing.
 Pub. Red. Xing.
 Br. 75.3-161' & 145' Ar.
 16' x 8' x 45' R.C. Br. Exh. H.A.L.A.
 952.7

Pr. Xing.
 Pub. Red. Xing.
 Br. 75.3-161' & 145' Ar.
 16' x 8' x 45' R.C. Br. Exh. H.A.L.A.
 952.7

79. E.B.
 $R=410'$
 $\Delta=136'09''$
 $E1.5''$
 $Sp=320'$

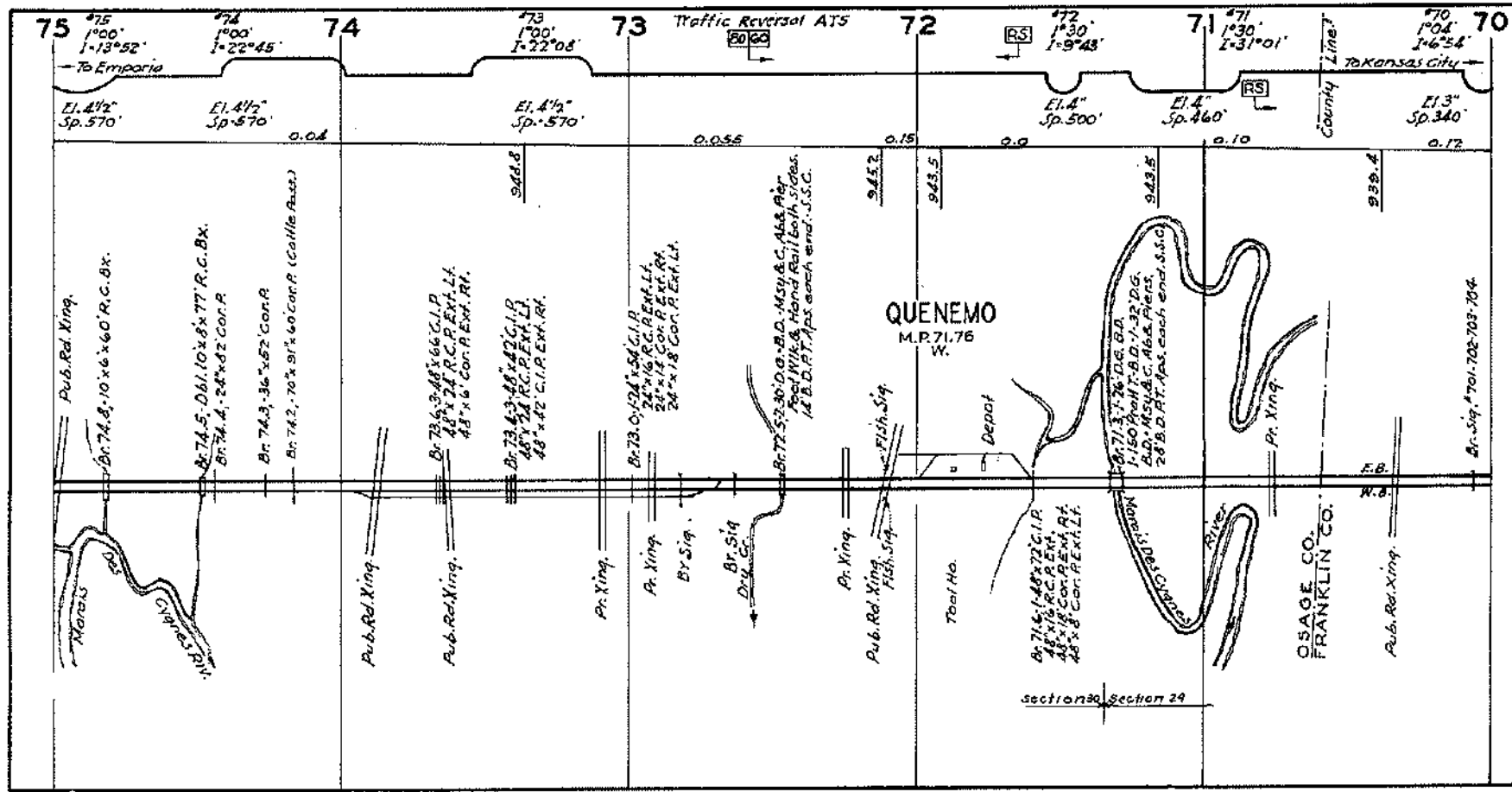
78
 $R=192'$
 $\Delta=74'$
 $E1.3''$
 $Sp=340'$

77
 $R=130'$
 $\Delta=21'59''$
 $E1.5''$
 $Sp=400'$

77 E1.3' Traffic Reversal A.T.S.
 $Sp=500'$
 $R=100'$
 $\Delta=13'04''$

To Kansas City

To Emporia



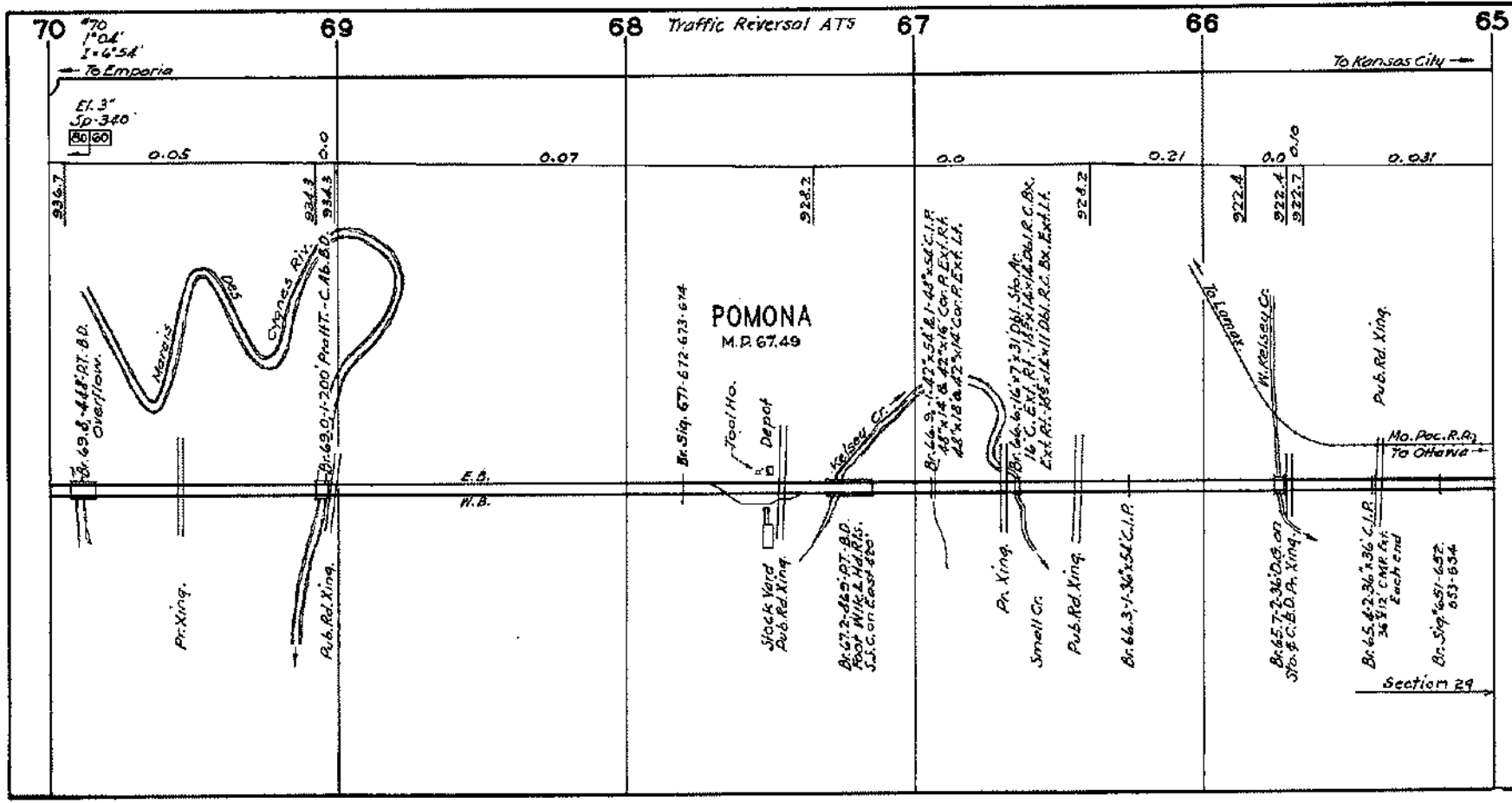
75 ⁷⁵ 1°00' 1-13°52' To Emporia
 74 ⁷⁴ 1°00' 1-22°45'
 73 ⁷³ 1°00' 1-22°08' Traffic Reversal ATS
 72 ⁷² 1°30' 1-9°43' RS
 71 ⁷¹ 1°30' 1-31°01' RS
 70 ⁷⁰ 1°04' 1-6°54' To Kansas City

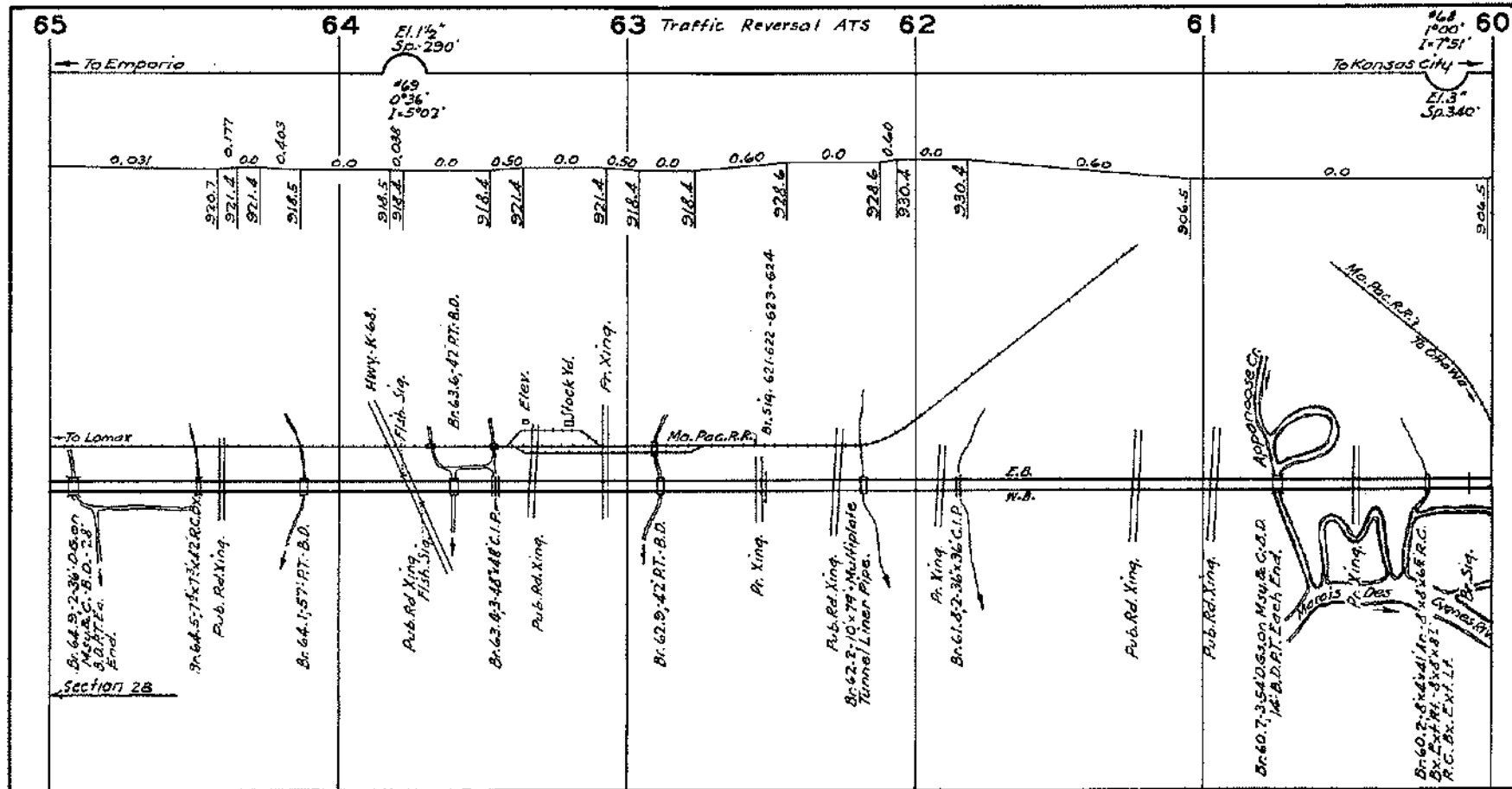
El. 412' Sp. 570' 0.04
 El. 412' Sp. 570'
 El. 4 1/2 Sp. 570' 0.056
 El. 4 1/2 Sp. 570' 0.15
 El. 4 Sp. 500' 0.9
 El. 4 Sp. 460' 0.10
 El. 3 Sp. 340' 0.17

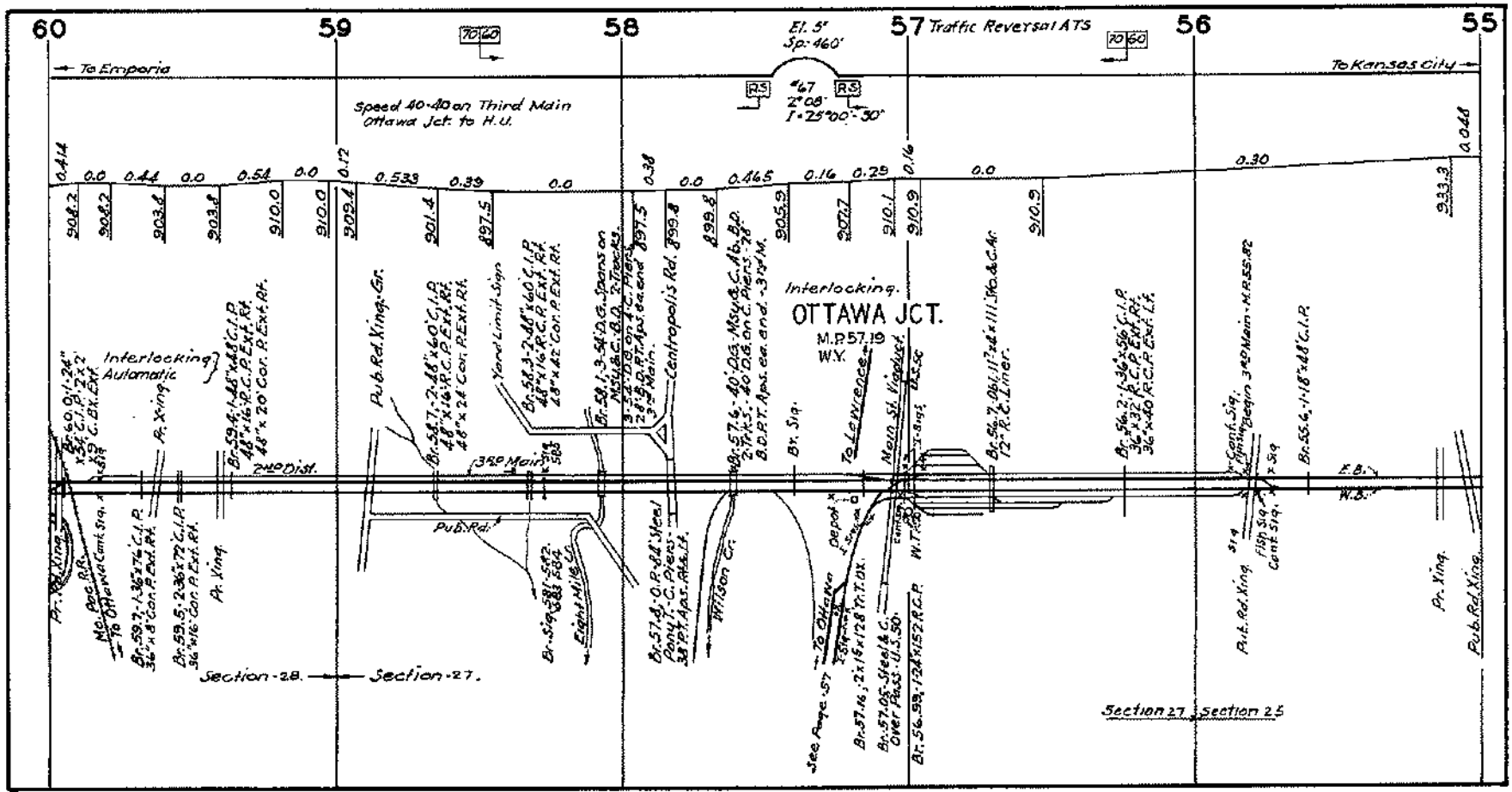
Pub. Rd. Xing.
 Br. 74.8, 10 x 60' R.C. B.X.
 Br. 74.5, 0.61.10 x 8 x 77' R.C. B.X.
 Br. 74.4, 28 x 62' Cor. R.
 Br. 74.3, 36 x 52' Cor. R.
 Br. 74.2, 70 x 91 x 60' Cor. R. (Cattle Rust.)
 Pub. Rd. Xing.
 Br. 73.6, 3.48 x 66' C.I.P.
 48 x 74' R.C. P. EXT. LT.
 43 x 6' Cor. R. EXT. RT.
 Br. 73.4, 3.88 x 42' C.I.P.
 48 x 74' R.C. P. EXT. LT.
 48 x 42' C.I.P. EXT. RT.
 Pub. Rd. Xing.
 Br. 73.0, 124 x 54' C.I.P.
 24 x 14' R.C. P. EXT. LT.
 24 x 14' Cor. R. EXT. RT.
 24 x 18' Cor. R. EXT. LT.
 Pub. Rd. Xing.
 Br. 72.5, 30' D.O. - B.D. - NSU & C. A. 68. P.R.
 Road Wk. & Horned Rail both slopes.
 18' B.D. RT. - App. each end. - S.S.C.
 Pub. Rd. Xing.
 Fish. Sig.
 Depot
 QuENEMO
 M.P. 71.76
 W.
 Pub. Rd. Xing.
 Fish. Sig.
 Toal Ho.
 Br. 71.6, 1.48 x 72' C.I.P.
 48 x 16' R.C. P. EXT. LT.
 48 x 18' Cor. R. EXT. RT.
 48 x 8' Cor. R. EXT. LT.
 Br. 71.3, 1.76 x 06' B.D.
 1-150' Road Wk. B.D. 1-32' D.G.
 B.D. - NSU & C. A. 68. P.R.
 28' B.D. RT. - App. each end. - S.S.C.
 Pub. Rd. Xing.
 F.B.
 N.B.
 Pub. Rd. Xing.
 Br. Sig. 701-702-703-704

section 23 section 24

OSAGE CO.
FRANKLIN CO.







60

59

58

57

56

55

To Emporia

To Kansas City

Speed 40-40 on Third Main
Ottawa Jct. to H.U.

El. 5'
Sp: 460'

Traffic Reversal ATS

0.414 0.0 0.44 0.0 0.54 0.0 0.17 0.533 0.39 0.0 0.38 0.0 0.465 0.16 0.29 0.16 0.0 0.30 0.048

208.2 208.2 203.f 203.f 210.0 210.0 209.4 201.4 197.5 199.8 207.7 210.1 210.9 210.9 233.3

Interlocking Automatic
A. Xing
Br. 59.4-1-48x48 C.I.P.
48"x48" R.C.P. EXT. RT.
48"x20" Cor. R. EXT. RT.
240 Dist.
Pub. Rd. King Cr.
Br. 58.7-2-48x60 C.I.P.
48"x48" R.C.P. EXT. RT.
48"x24" Cor. R. EXT. RT.
Yard Limit Sign
Br. 58.3-2-48x60 C.I.P.
48"x48" R.C.P. EXT. RT.
48"x48" Cor. R. EXT. RT.
Br. 58.1-3-54x6 Spans on
Main St. 18' 0" Truss
3' 2" Dia. 4' 0" High
28' 9" 27' 11/2" Sp. ea. end 897.5
3' 2" Main.
Centropolis Rd. 899.8
299.8
Br. 57.6-40' Dia. M.S. & C. Ab. B.P.
2 Trks. 40' Dia. on C. M.S. 28
B.O.P.T. Apr. ea. end. -33' 1/2" M.
Br. Sig.
Interlocking
OTTAWA JCT.
M.P. 57.19
WX
To Lawrence
Main St. Viaduct
Br. 56.7-1-36x56 C.I.P.
36"x36" R.C.P. EXT. RT.
36"x40" R.C.P. EXT. LT.
Br. 56.7-1-36x56 C.I.P.
36"x36" R.C.P. EXT. RT.
36"x40" R.C.P. EXT. LT.
Br. 56.6-1-18x48 C.I.P.
F.B.
N.S.
Pub. Rd. King Cr.
Fish Sig. 814
Cent. Sig. 717 89

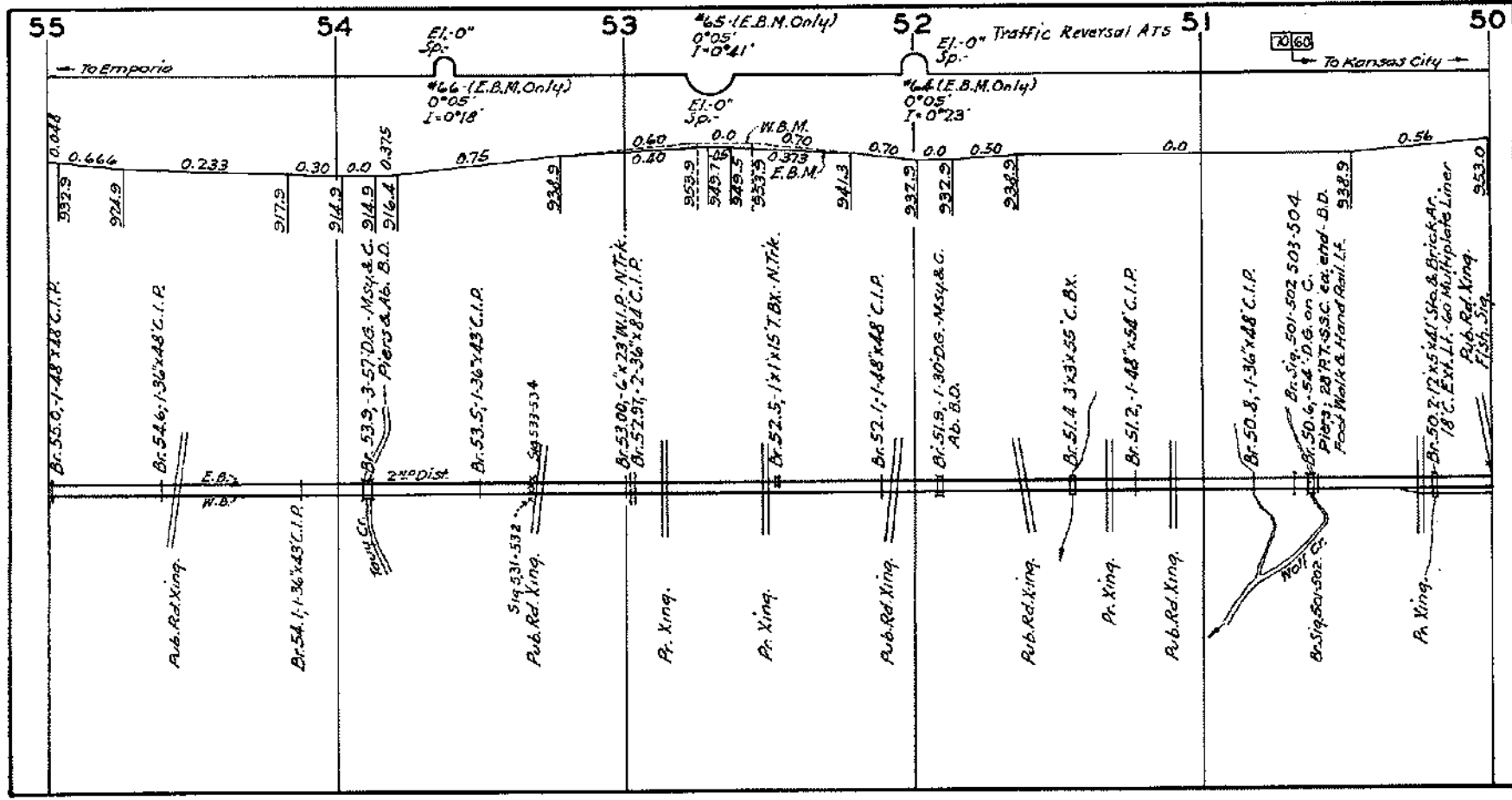
Section 28 Section 27

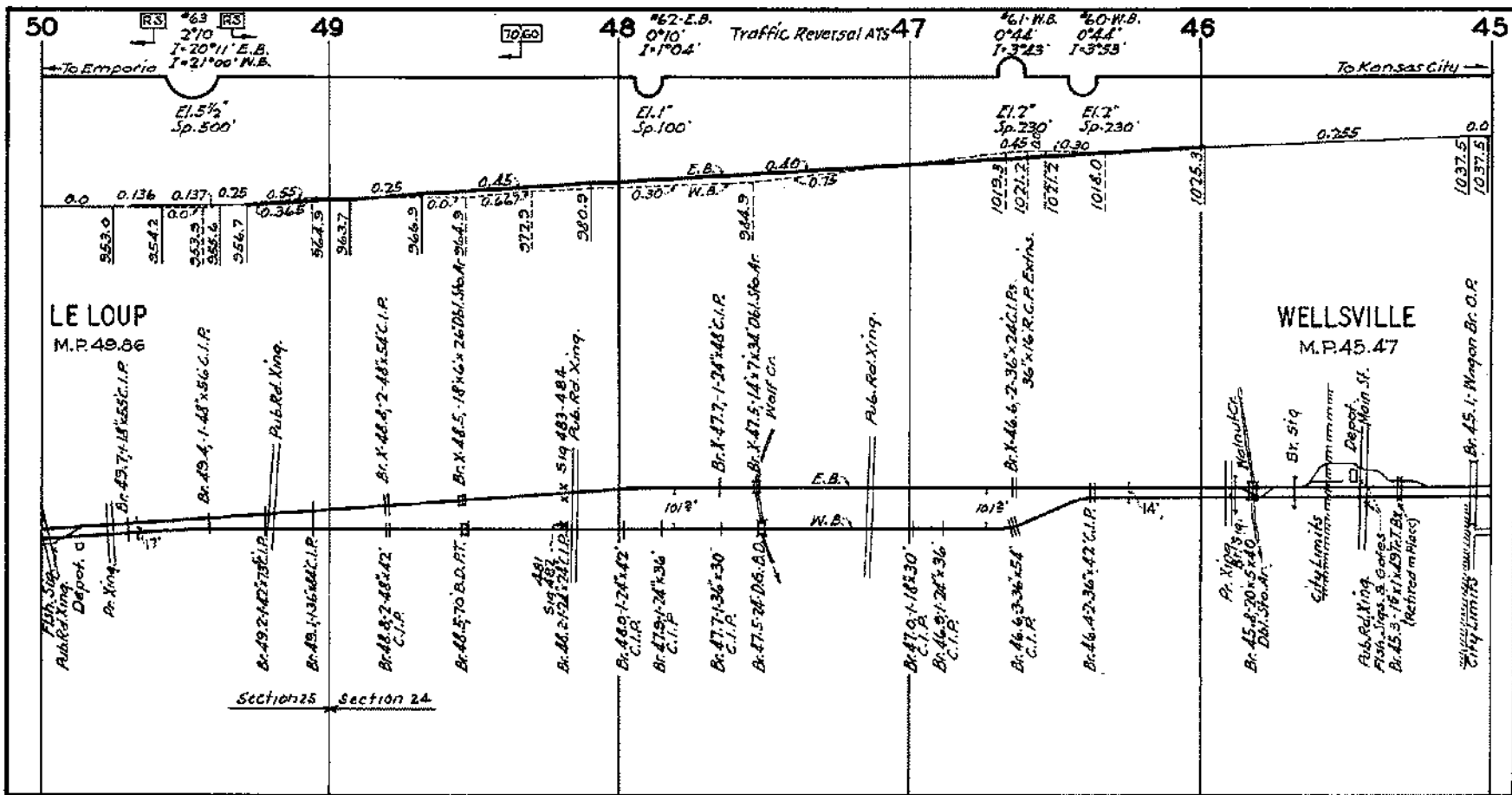
Section 27 Section 25

See Page 57 To Ottawa Depot

Br. 57.16-2x18x128 Tr. Trk.
Br. 57.05-Steel & C.
Over Pass-U.S.S.C.
Br. 56.99-12x152 R.C.P. W.T. 28

Ph. Xing
Pub. Rd. King Cr.





50

49

48

47

46

45

RS #63
2°10'
I-20°11' E.B.
I-21°00' N.B.

70.60

#62-E.B.
0°10'
I-1°04'

Traffic Reversal ATS

#61-N.B.
0°44'
I-3°23'

#60-N.B.
0°44'
I-3°53'

To Kansas City

E.L. 5 1/2'
Sp. 500'

E.L. 1'
Sp. 100'

E.L. 2'
Sp. 230'

E.L. 2'
Sp. 230'

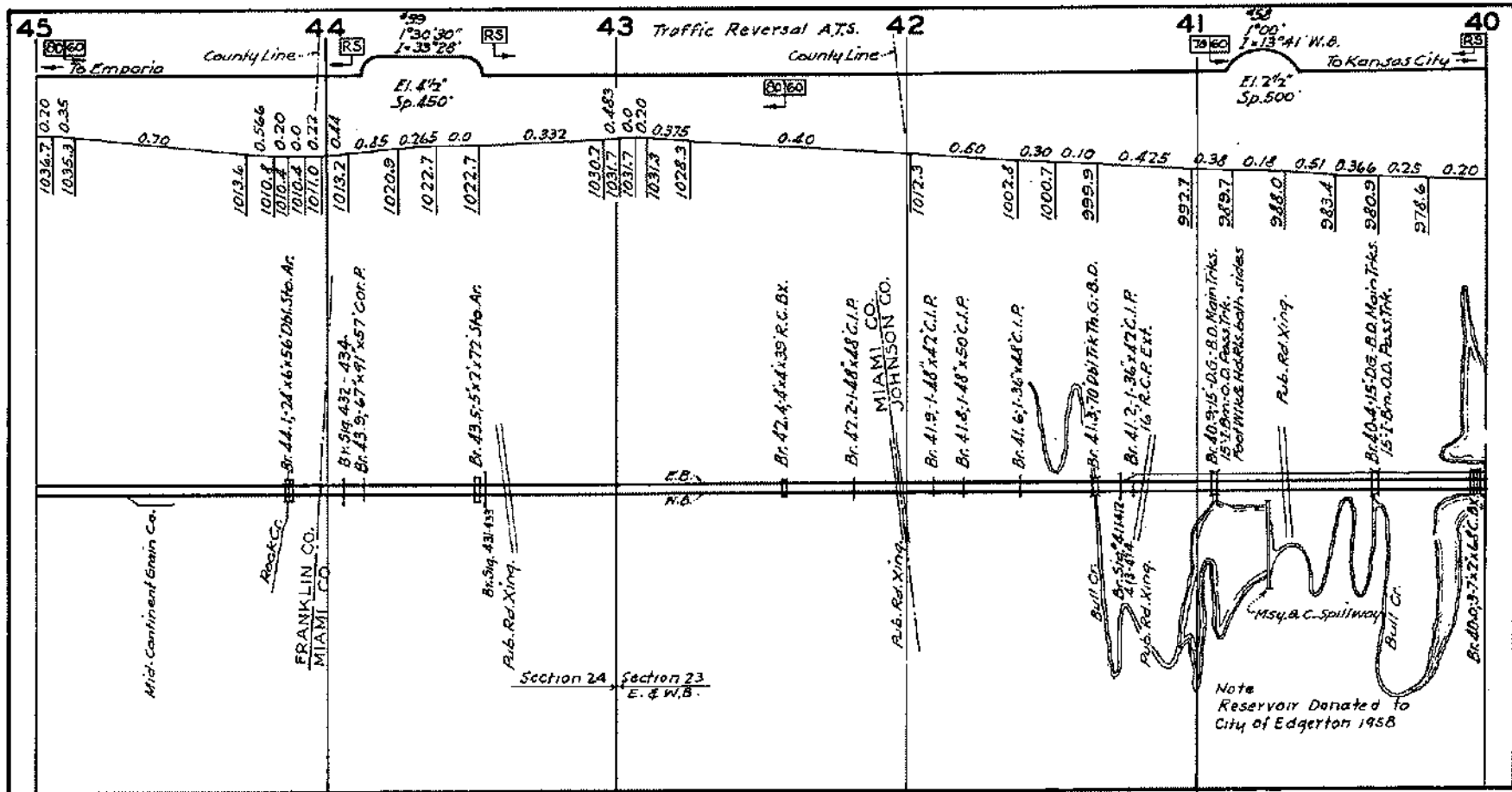
0.0 0.136 0.137 0.25 0.55 0.368 0.25 0.45 0.625 0.0 0.30 0.40 0.75 1019.9 1021.2 1027.7 1018.0 1025.3 0.255 0.0 1037.5 1037.5

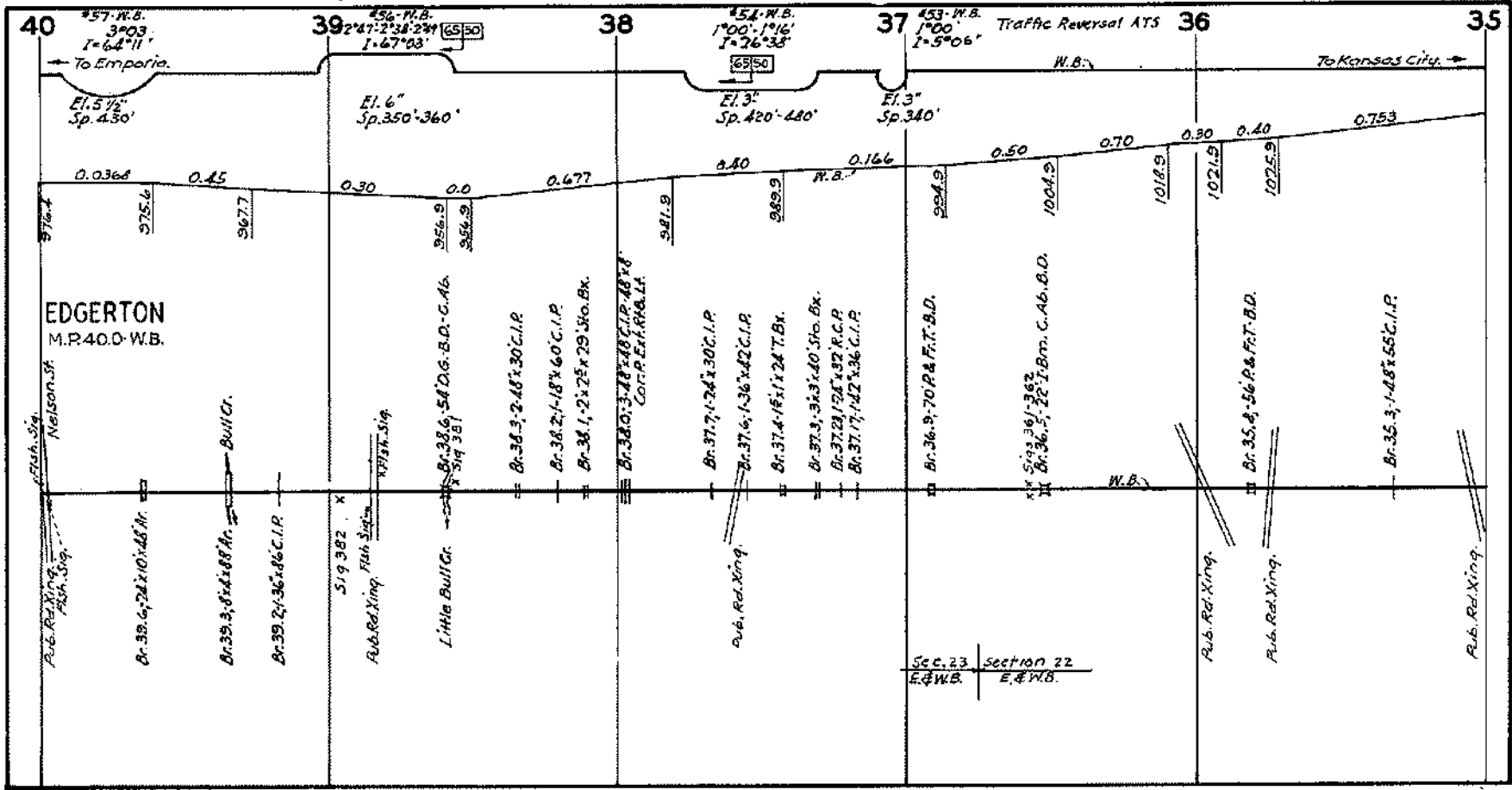
LE LOUP
M.P. 49.86

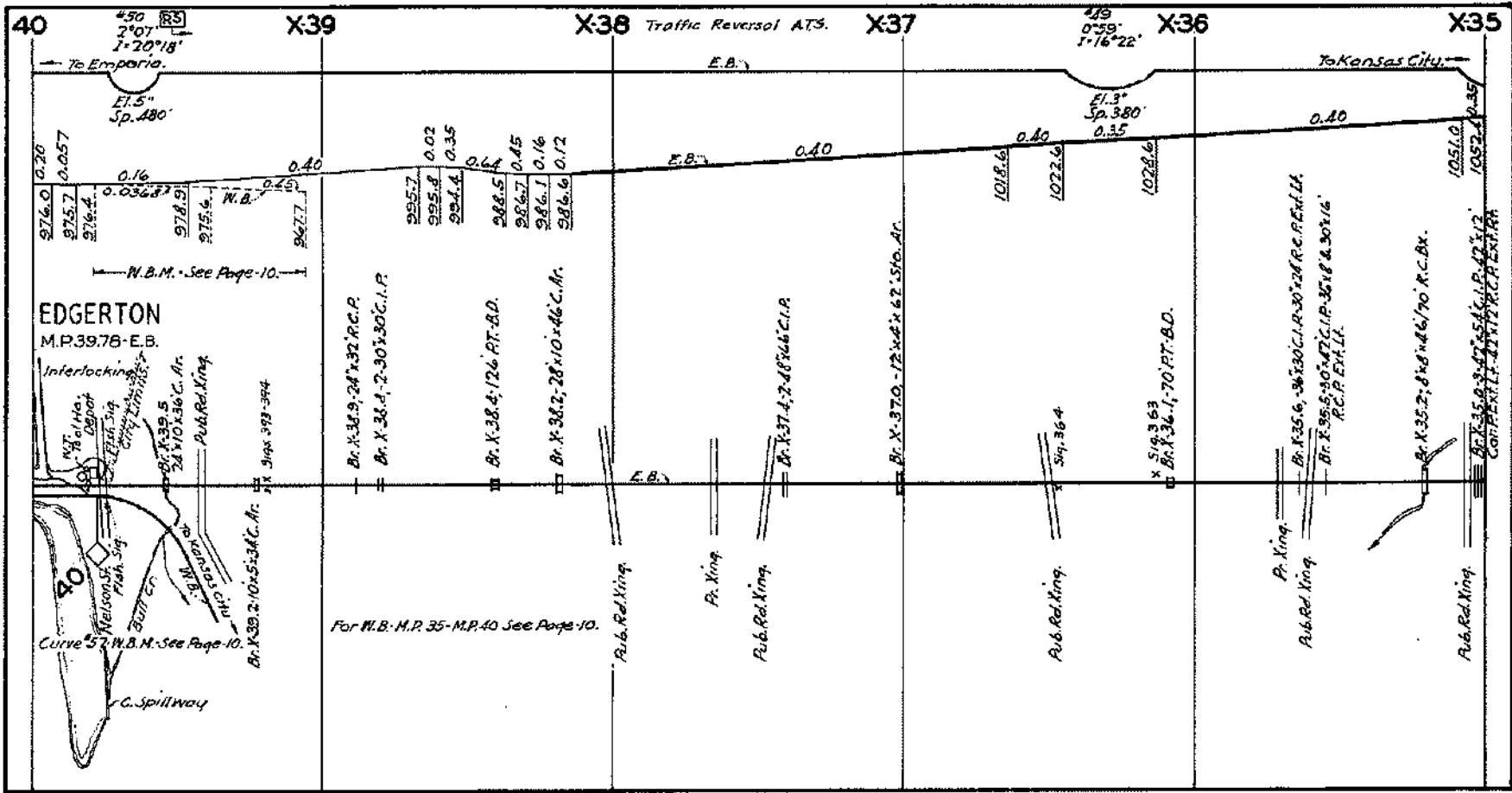
WELLSVILLE
M.P. 45.47

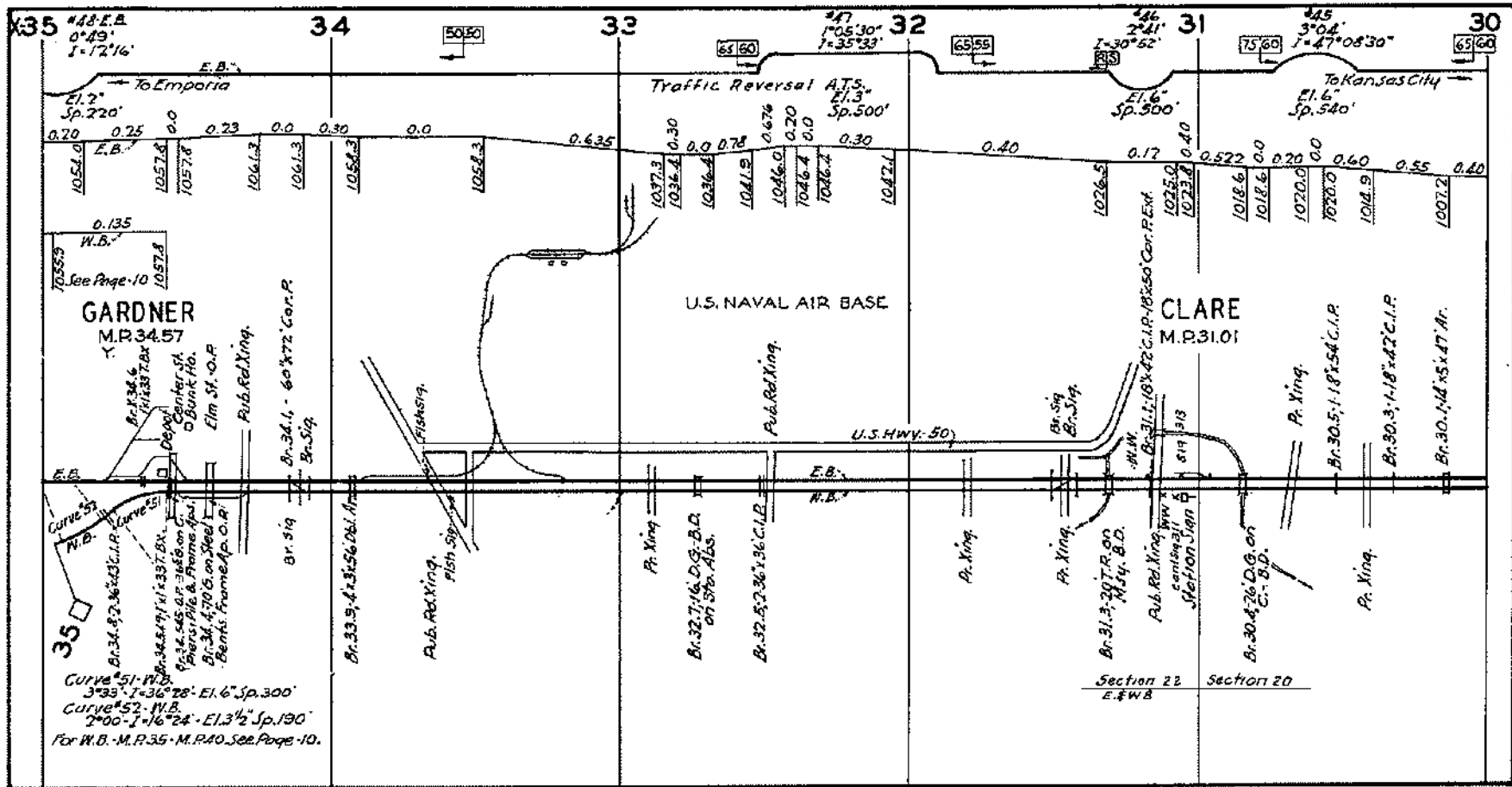
Pub. Red. Xing. Depot. Fish. Sign. Br. 49.74-18°55' C.I.P. Br. 49.4-1-48' x 86' C.I.P. Pub. Red. Xing. Br. 49.2-1-47' x 75' C.I.P. Br. 49.1-1-36' x 62' C.I.P. Br. 48.8-2-48' x 42' C.I.P. Br. 48.5-70' B.D. PT. Br. 48.1-2-41' x 42' C.I.P. Sig. 487 Br. 48.2-1-24' x 24' C.I.P. x Sig 483-484 Pub. Red. Xing. Br. 48.0-1-24' x 42' C.I.P. Br. 47.9-1-24' x 36' C.I.P. Br. 47.7-1-24' x 48' C.I.P. Br. 47.5-24' D.G. B.D. Wolf Cr. Pub. Red. Xing. Br. 47.0-1-18' x 30' C.I.P. Br. 46.9-1-24' x 36' C.I.P. Br. 46.6-3-36' x 54' C.I.P. Br. 46.4-2-36' x 42' C.I.P. Br. 45.9-1-19' x 40' Dbl. Sp. N. Br. 45.8-1-19' x 40' Dbl. Sp. N. Fish. Sign. Pub. Red. Xing. Depot. Main St. Fish. Sign. & Gates Br. 45.3-18' x 19' T. Br. (Retired m. Place) City Limits Br. 45.1-1-19' x 40' Dbl. Sp. N. City Limits Br. 45.1-1-19' x 40' Dbl. Sp. N. Br. 45.1-1-19' x 40' Dbl. Sp. N. Br. 45.1-1-19' x 40' Dbl. Sp. N.

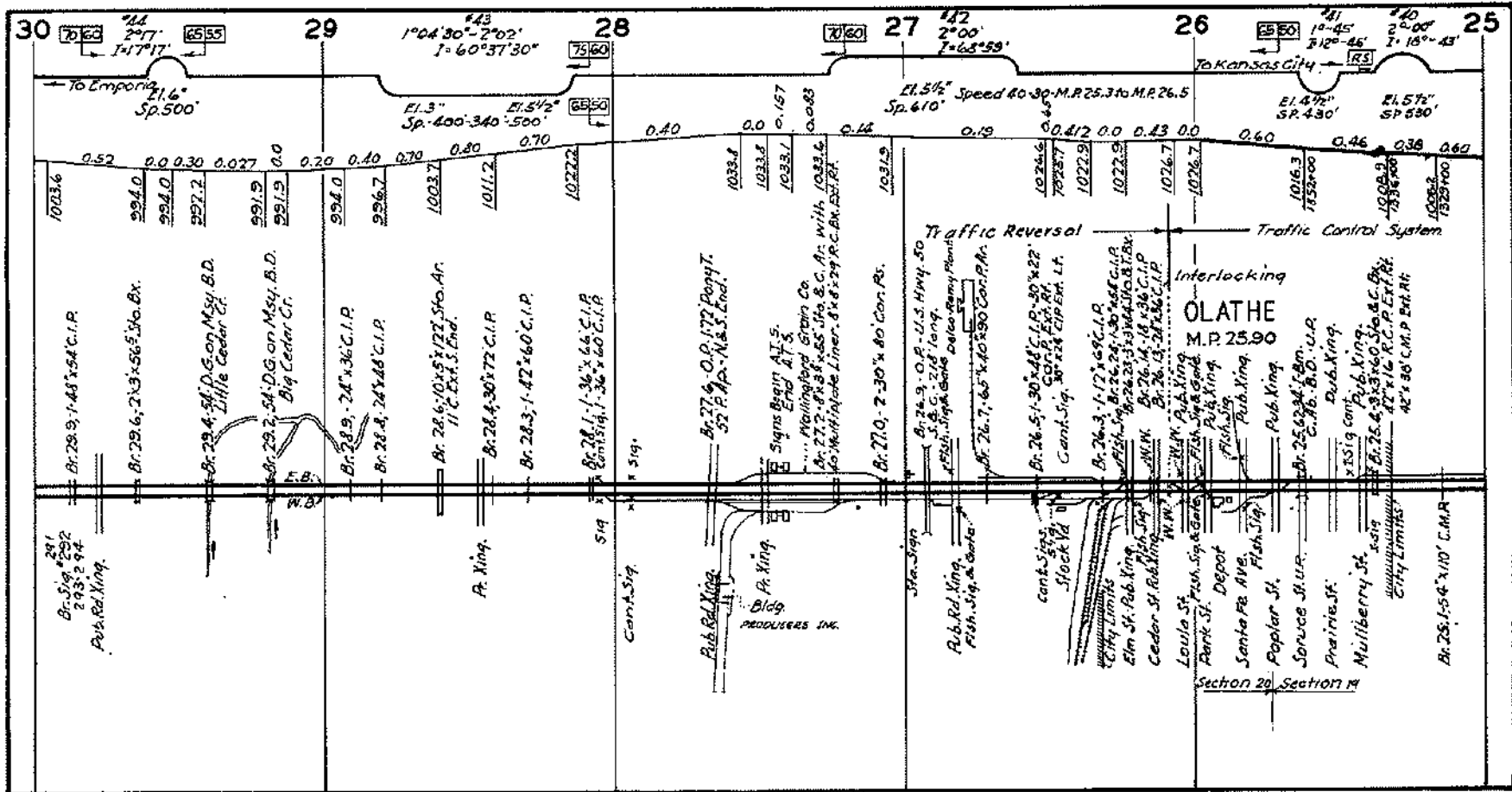
Section 25 Section 24











Br. Sig. #292
2' x 3' x 4'

Pub. Rd. Xing.

Br. 28.9, 1-43 x 64' C.I.P.

Br. 29.6, 2' x 3' x 56' 5/8' Br.

Br. 29.4, 34' D.G. on Msy. B.D.
Little Cedar Cr.

Br. 29.2, 54' D.G. on Msy. B.D.
Big Cedar Cr.

Br. 28.9, 24' x 36' C.I.P.

Br. 28.8, 24' x 48' C.I.P.

Br. 28.6, 10' x 5' x 72' Sto. Ar.
11' C. Ext. S. End.

Br. 28.4, 30' x 72' C.I.P.

Br. 28.3, 1-42' x 60' C.I.P.

Br. 28.1, 1-36' x 66' C.I.P.
Com. Sig. 1' 36' x 60' C.I.P.

Pub. Rd. Xing.
Bldg.
PRODUCERS INC.

Signs Begin A.I.S.
End A.I.S.
Wallford Grain Co.
Ar. 27.2, 8' x 8' x 55' Sto & C. Ar. with 1033.6
No. 11' x 14' Gate Limer. 8' x 8' x 29' R.C. B.M. Ext. Rt.

Br. 27.0, 2-30' x 80' Con. R.R.

Traffic Reversal

Traffic Control System

Interlocking

OLATHE
M.P. 25.90

Br. 26.9, O.P. U.S. Hwy. 50
J. B. C. 218' Long.
Fish. Sig. Gate Delta Army Plant

Br. 26.7, 65' x 40' 300' Con. P.A.

Br. 26.5, 1-30' x 48' C.I.P. 30' x 22'
Comp. Ext. Rt.
Cont. Sig. 30' x 24' C.I.P. Ext. Lt.

Br. 26.3, 1-17' x 69' C.I.P.

Br. 26.2, 24' x 30' x 6' C.I.P.

Br. 26.2, 3' x 3' x 4' 1/2' B.T.B.

Br. 26.1, 18' x 36' C.I.P.

Br. 26.1, 13' x 24' x 36' C.I.P.

Br. 26.1, 11' x 11' x 11' Pub. Xing.

Fish. Sig. & Gate
Pub. Xing.
Fish. Sig.
Pub. Xing.
Pub. Xing.

Br. 25.6, 24' x 80' C.A.B. B.D. U.P.

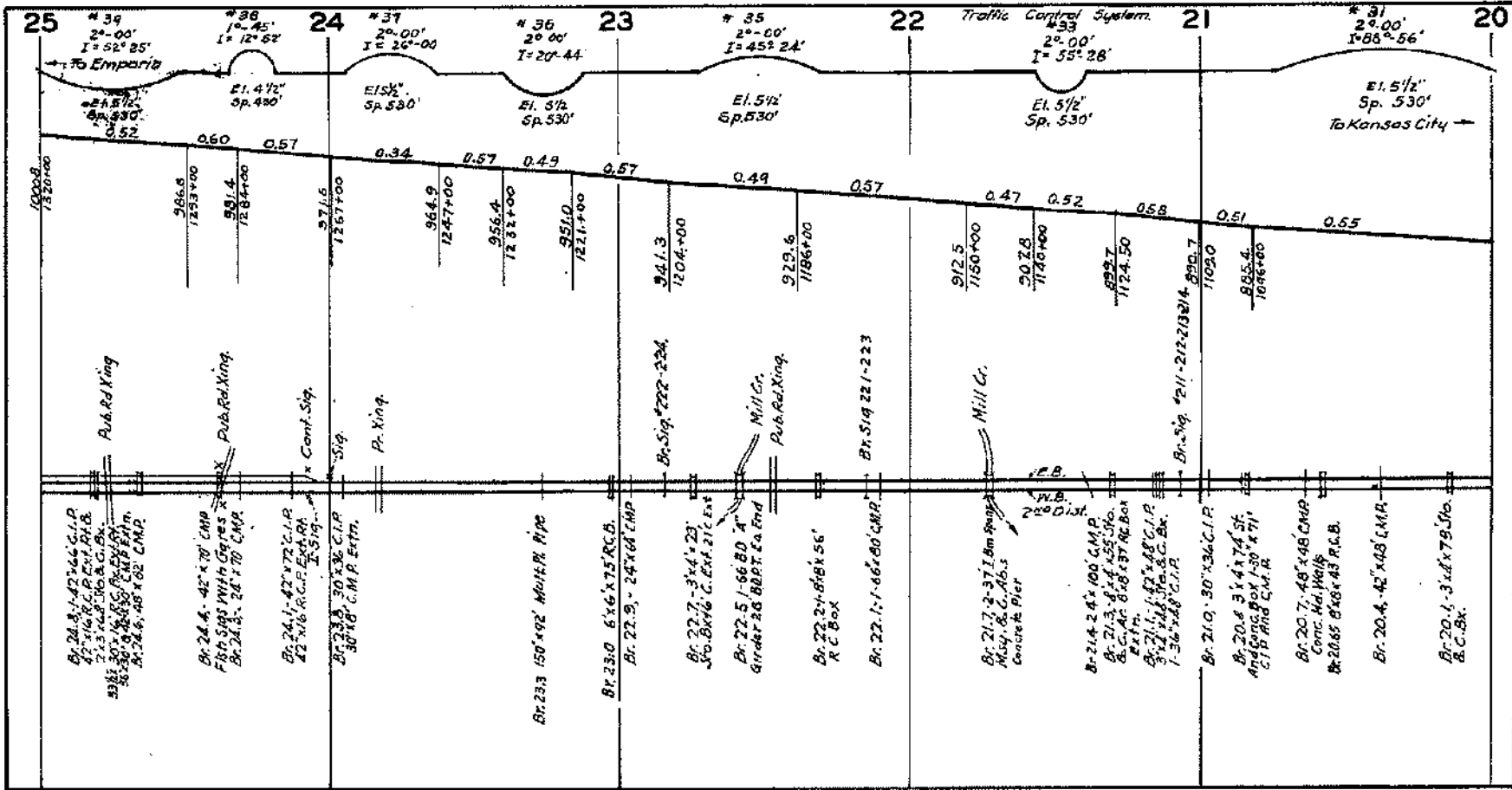
Br. 25.6, 24' x 80' C.A.B. B.D. U.P.
cont. Pub. Xing.

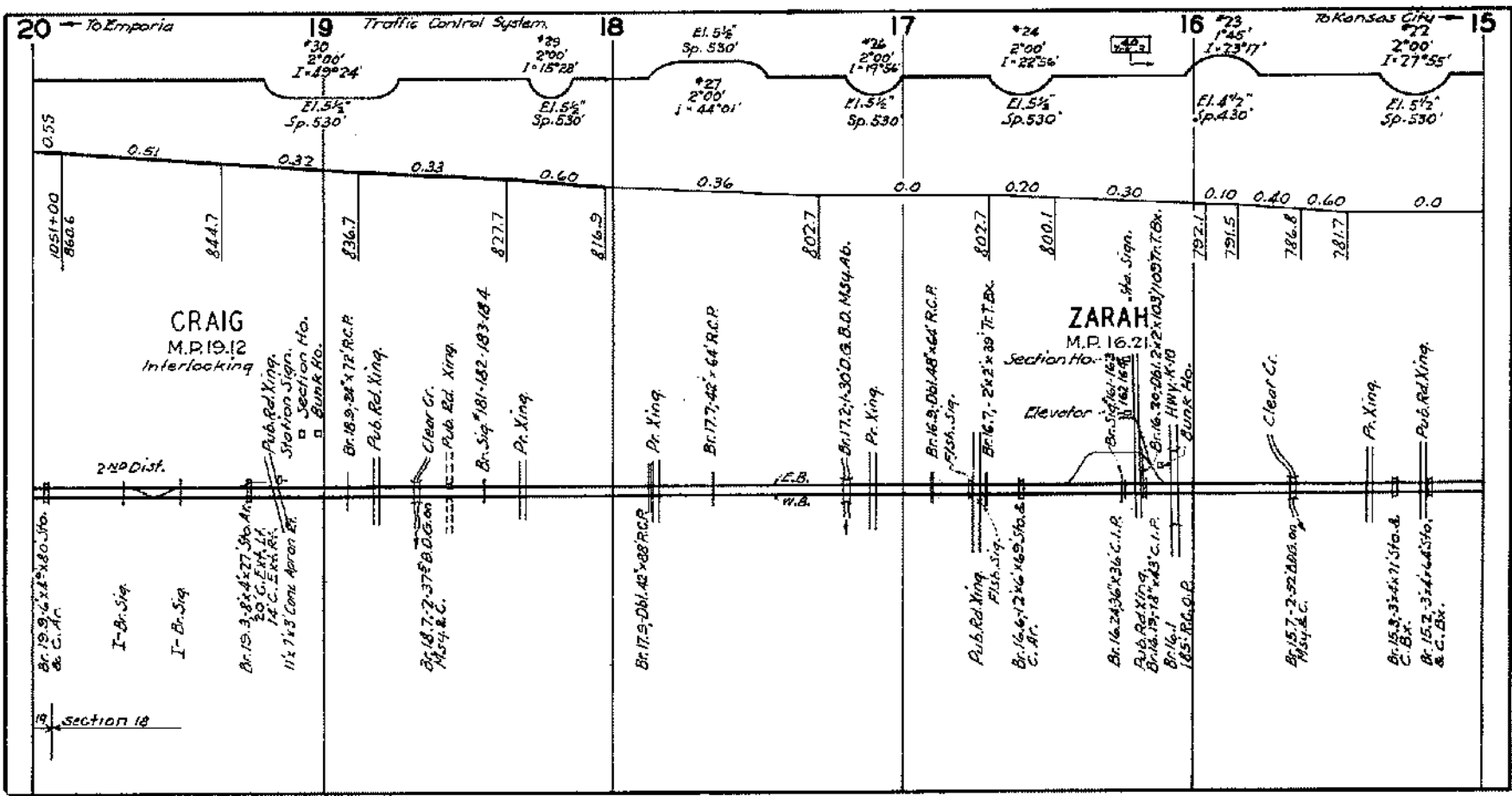
Br. 25.5, 2' x 3' x 60' Sto. & C. Br.
42' x 16' R.C. P. Ext. Rt.
42' x 80' C.M.P. Ext. Rt.

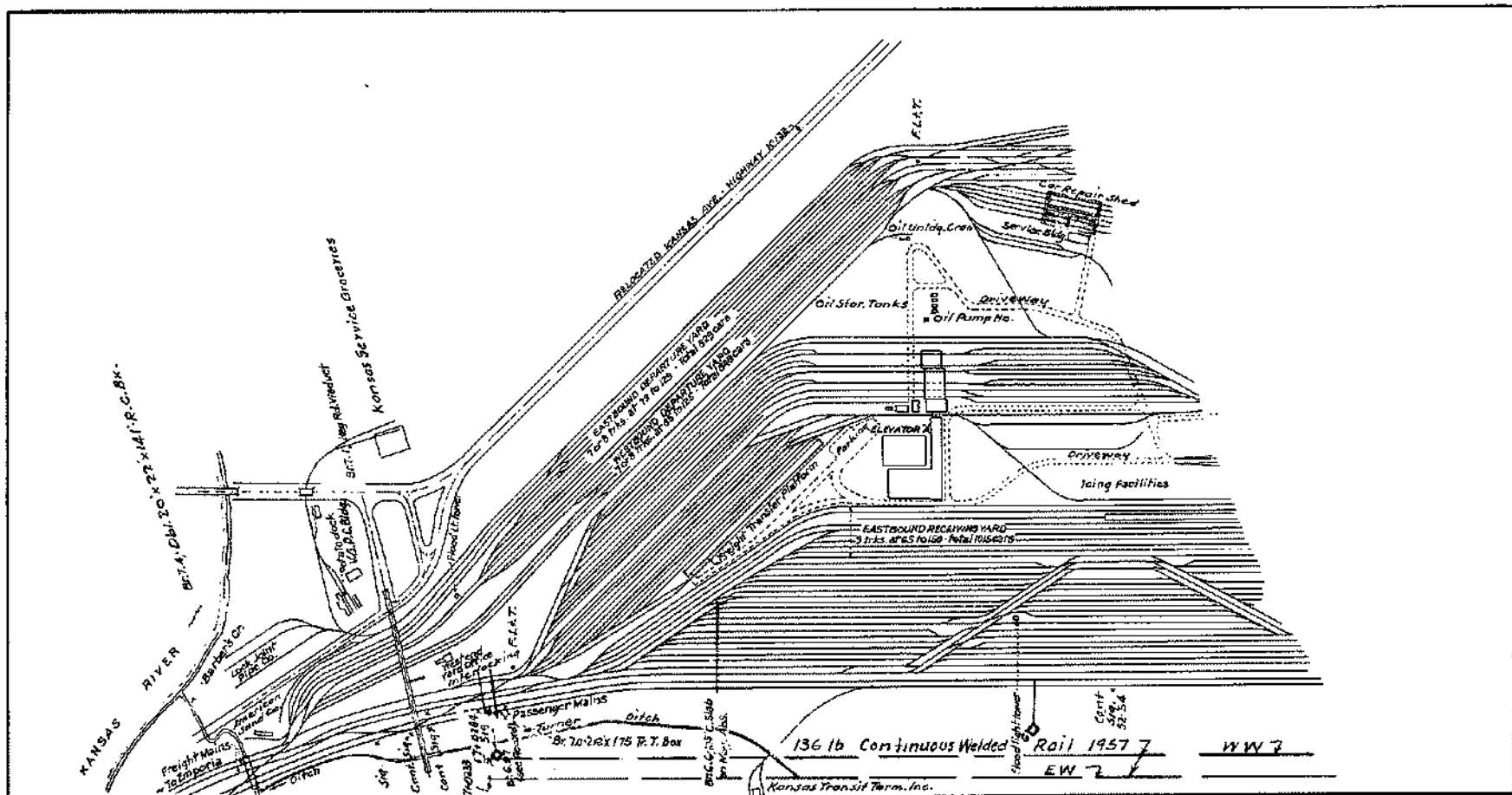
City Limits
Elm St. Pub. Xing.
Fish. Sig.
Cedar St. Pub. Xing.
W.W.
Louisa St.
Park St.
Santa Fe Ave.
Poplar St.
Service St. U.P.
Prairie St.
Mulberry St.
City Limits

Section 20 Section 18

Br. 25.1, 54' x 110' C.M.R.



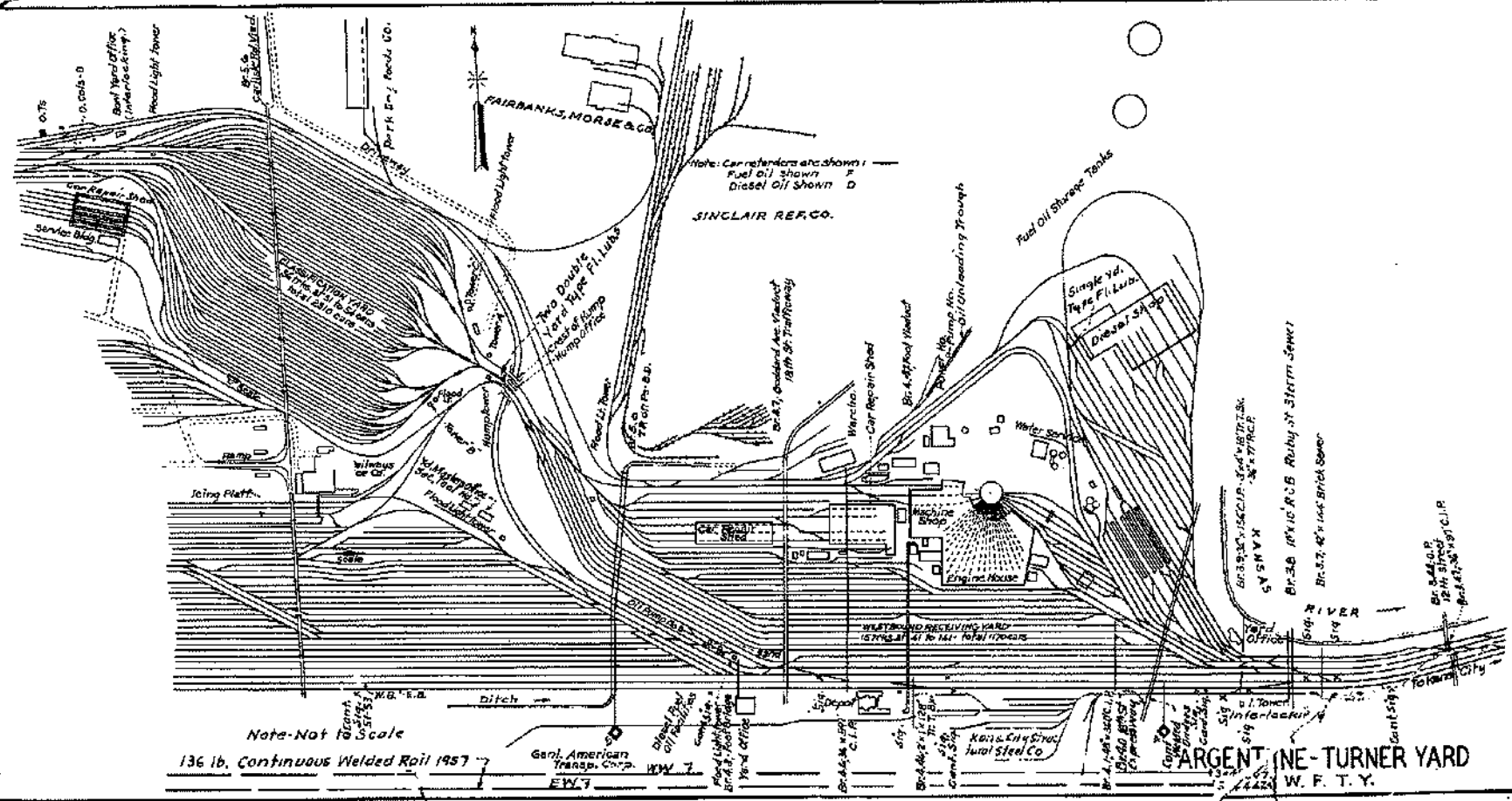




136 lb Continuous Welded Rail 1957

EW 7 WW 7

Kansas Transit Term. Inc.



0.75
0.0015-D
Bent Head Office
(Interlocking)
Head Light Tower

FAIRBANKS, MORSE & CO

SINCLAIR REC. CO.

Note: Car referencs are shown:
Fuel oil shown F
Diesel oil shown D

Fuel Oil Storage Tanks

Single Yd.
Type P.I. LUBS
Diesel Shop

BR-30 40'x10' R.C.B. Ruby St. Storm Sewer

BR-37 40'x10' Brick Spore

BR-35 30'x15' SECIP - 36" x 18" x 21" BK.

SY S.W.V.

BR-34 40'x10' P
12' in. street
BR-34 30'x15' SECIP

RIVER

54' 514'

514'

514'

514'

514'

514'

514'

Note-Not to Scale

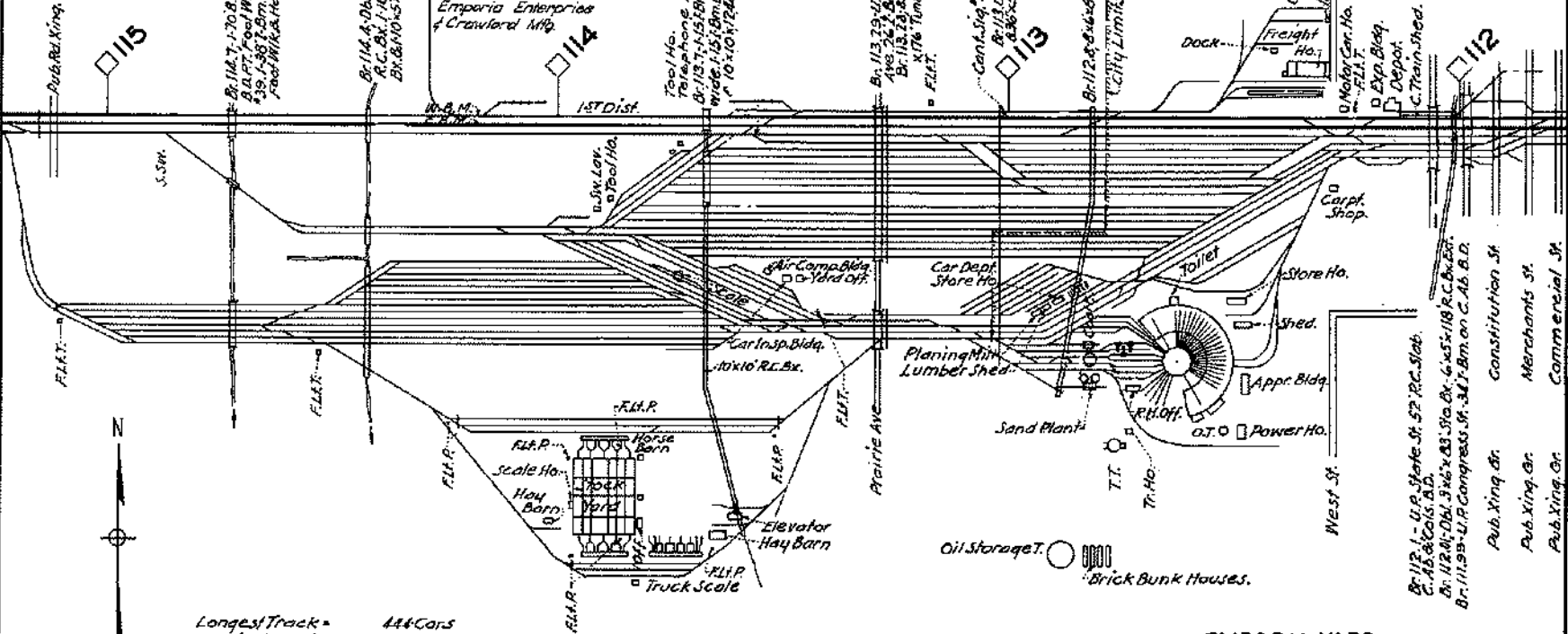
136 lb. Continuous Welded Rail 1957

Genl. American
Transp. Corp.
E.V. 7

ARGENT (NE-TURNER YARD)
W. F. T. Y.

← To Newton
MERRICK
M.P. 115.30
Interlocking.

To Topeka →
EMPORIA
M.P. 112.13
W.F.T.V.



Signals Shown on Page 54 only
Emporia Enterprises
& Crawford Mfg.

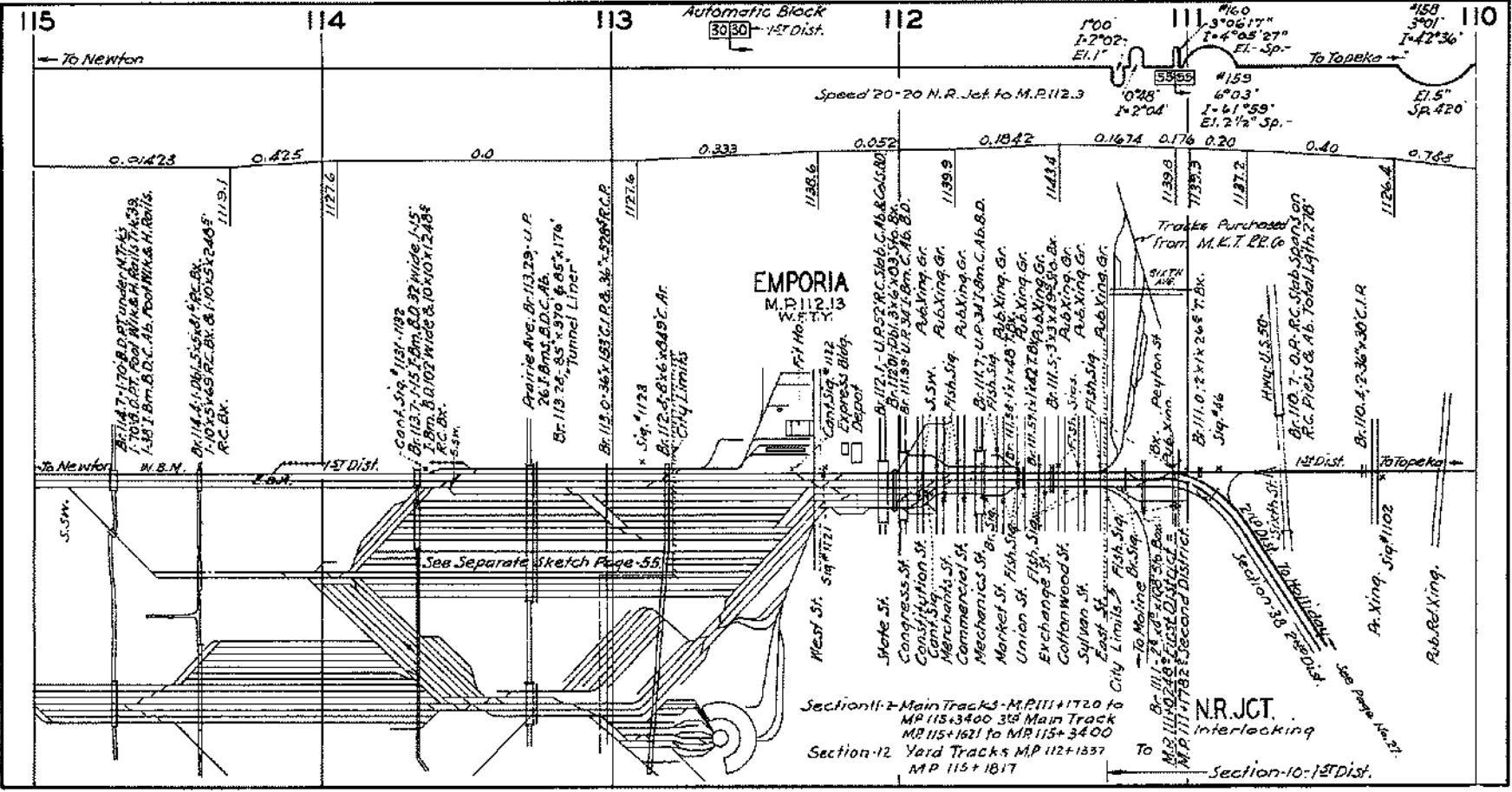
Longest Track = 444 Cars
Shortest Track = 2 Cars
Total Car Capacity = 4930 Cars

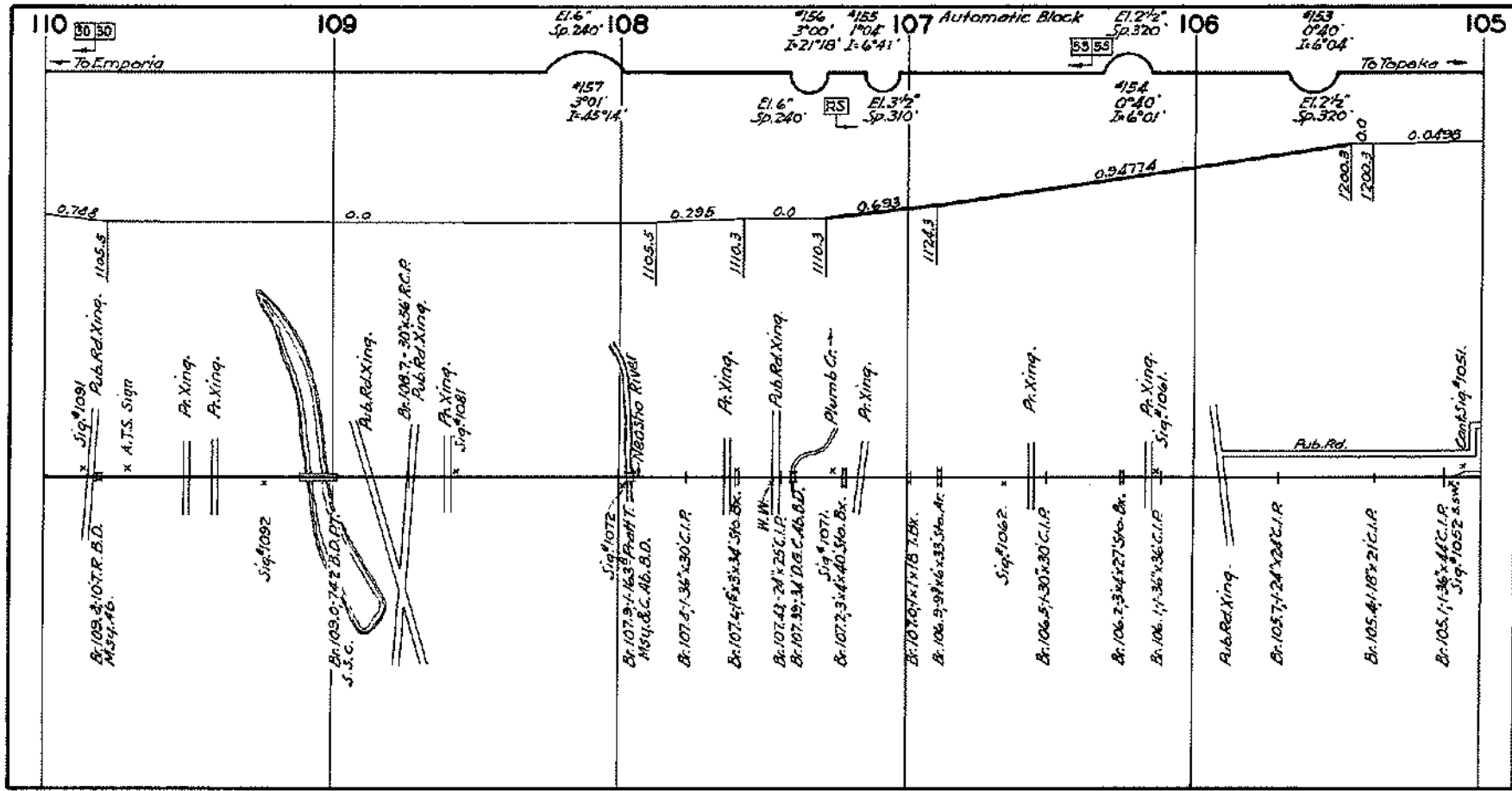
Note: Not to Scale.

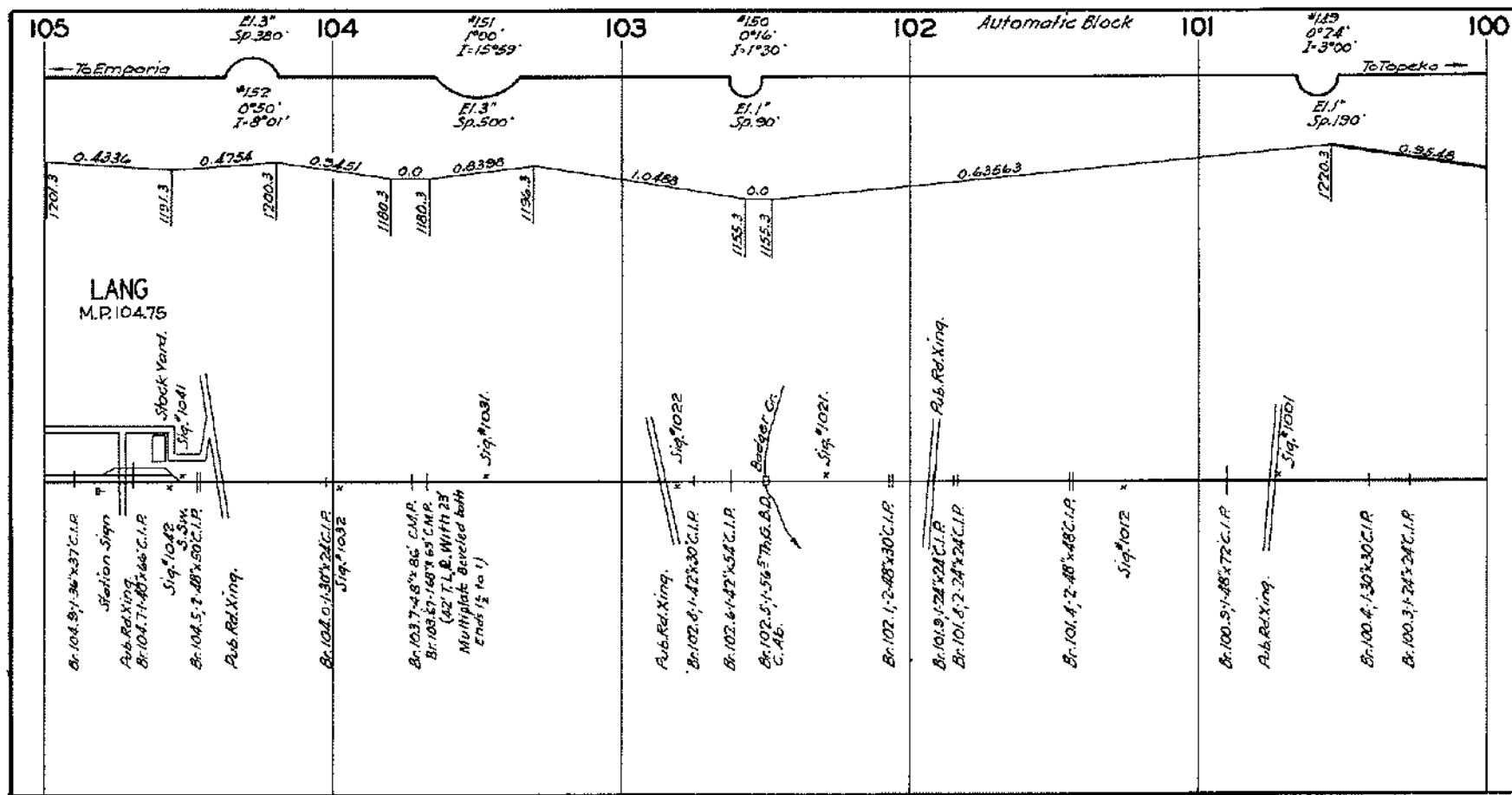
EMPORIA YARD
EMPORIA, KANS.

Br. 112-1 - U.P. State St. 52' R.C. Bldg.
C. 115-6-118-119-120 - U.P. 3' 6" x 8' 6" Sta. Bx. 4' x 5' 11/8" R.C. Bldg.
Br. 111-99 - U.P. Congress St. 347' Bm. on C. Ab. B.D.

Brick Bunk Houses.







105

EI. 3" Sp. 320'

104

#151
100'
I=15°59'

103

#150
0°16'
I=1°30'

102

Automatic Block

101

#149
0°24'
I=3°00'

100

To Emporia

To Topeka

#152
0°50'
I=8°01'

EI. 3" Sp. 500'

EI. 1" Sp. 90'

EI. 1" Sp. 190'

0.4336

0.4754

0.5451

0.0

0.6398

1.0488

0.0

0.63563

0.9548

720.3

1191.3

1200.3

1190.3

1180.3

1196.3

1155.3

1155.3

1220.3

LANG
M.P. 104.75

Stock Yard

Stock Yard

Br. 104.9; 36'x37' C.I.P.

Station Sign

Pub. Ret. Xing.

Br. 104.7; 40'x44' C.I.P.

Sign #1042

Br. 104.5; 24'x50' C.I.P.

Pub. Ret. Xing.

Br. 104.0; 30'x24' C.I.P.

Sign #1032

Br. 103.7; 48'x86' C.M.P.

Br. 103.6; 148'x69' C.M.P.

(42' T.L.R. WITH 23'

Multiple derived both

ends 1/2 to 1)

Sign #1031

Pub. Ret. Xing.

Sign #1022

Br. 102.6; 42'x30' C.I.P.

Br. 102.6; 42'x54' C.I.P.

Br. 102.5; 56'x74' B.D. C.A.B.

Badger Cr.

Sign #1021

Br. 102.1; 248'x30' C.I.P.

Br. 101.9; 24'x24' C.I.P.

Br. 101.6; 24'x24' C.I.P.

Pub. Ret. Xing.

Br. 101.4; 248'x48' C.I.P.

Sign #1012

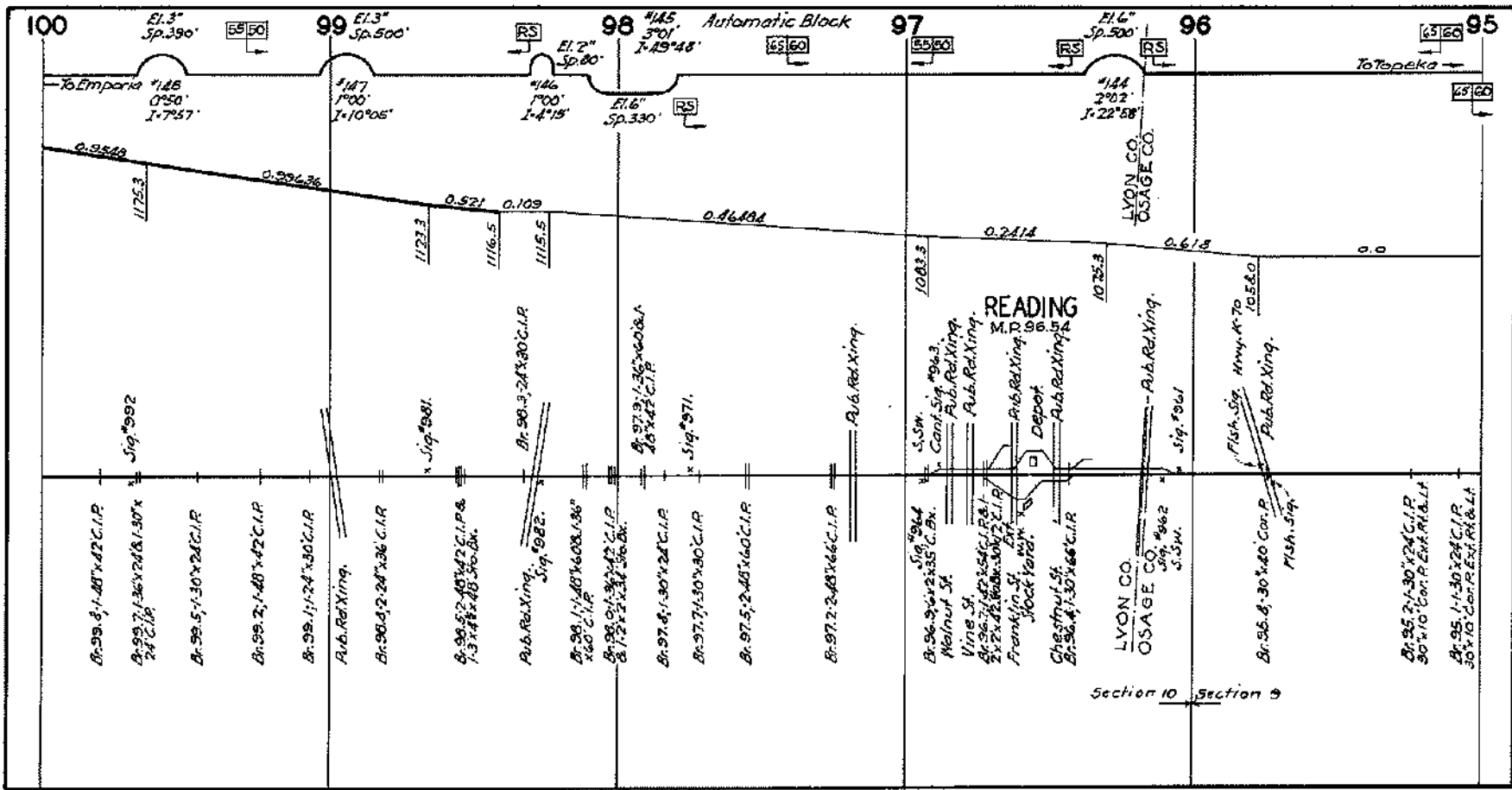
Br. 100.9; 48'x72' C.I.P.

Pub. Ret. Xing.

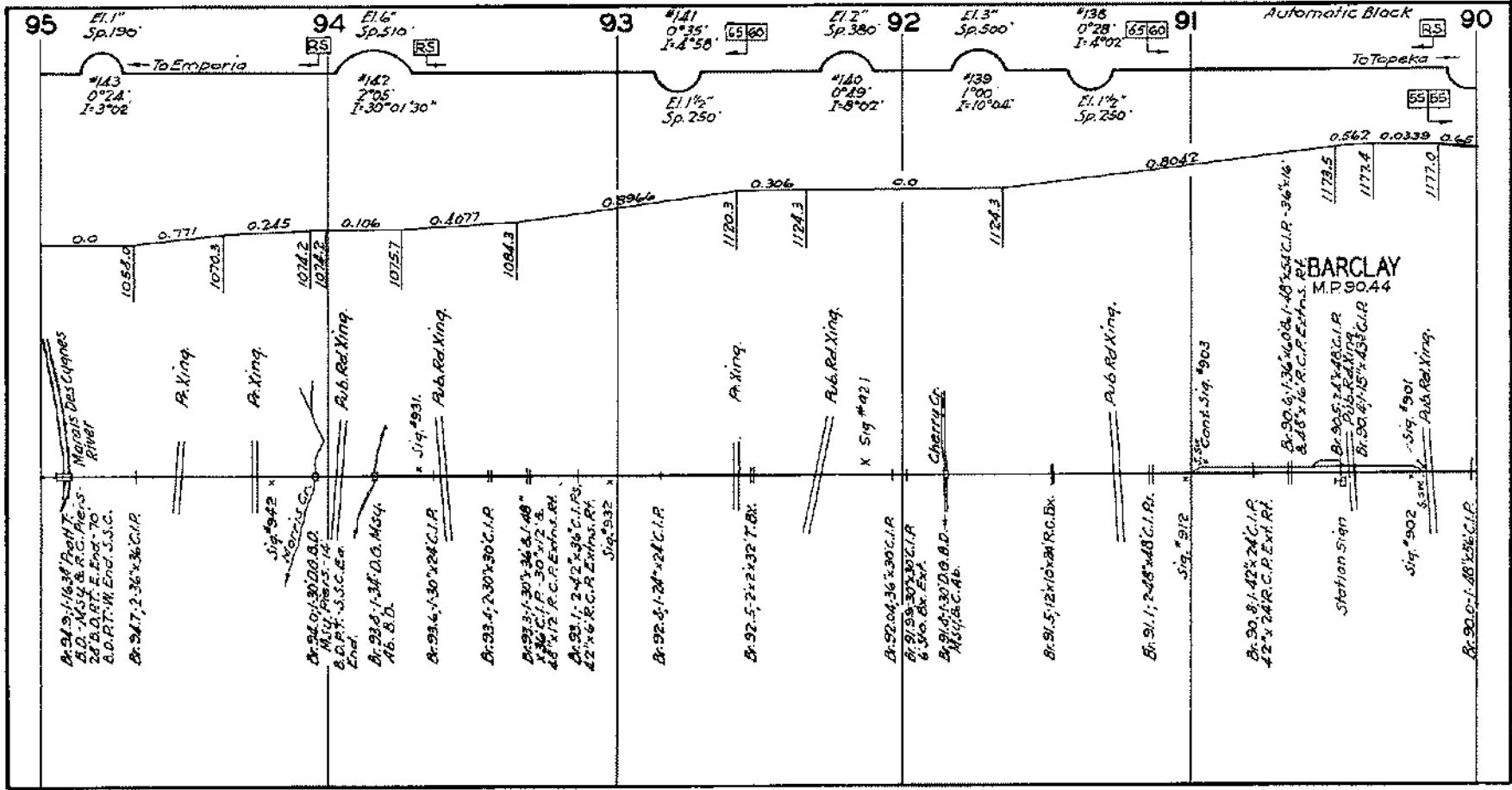
Sign #1001

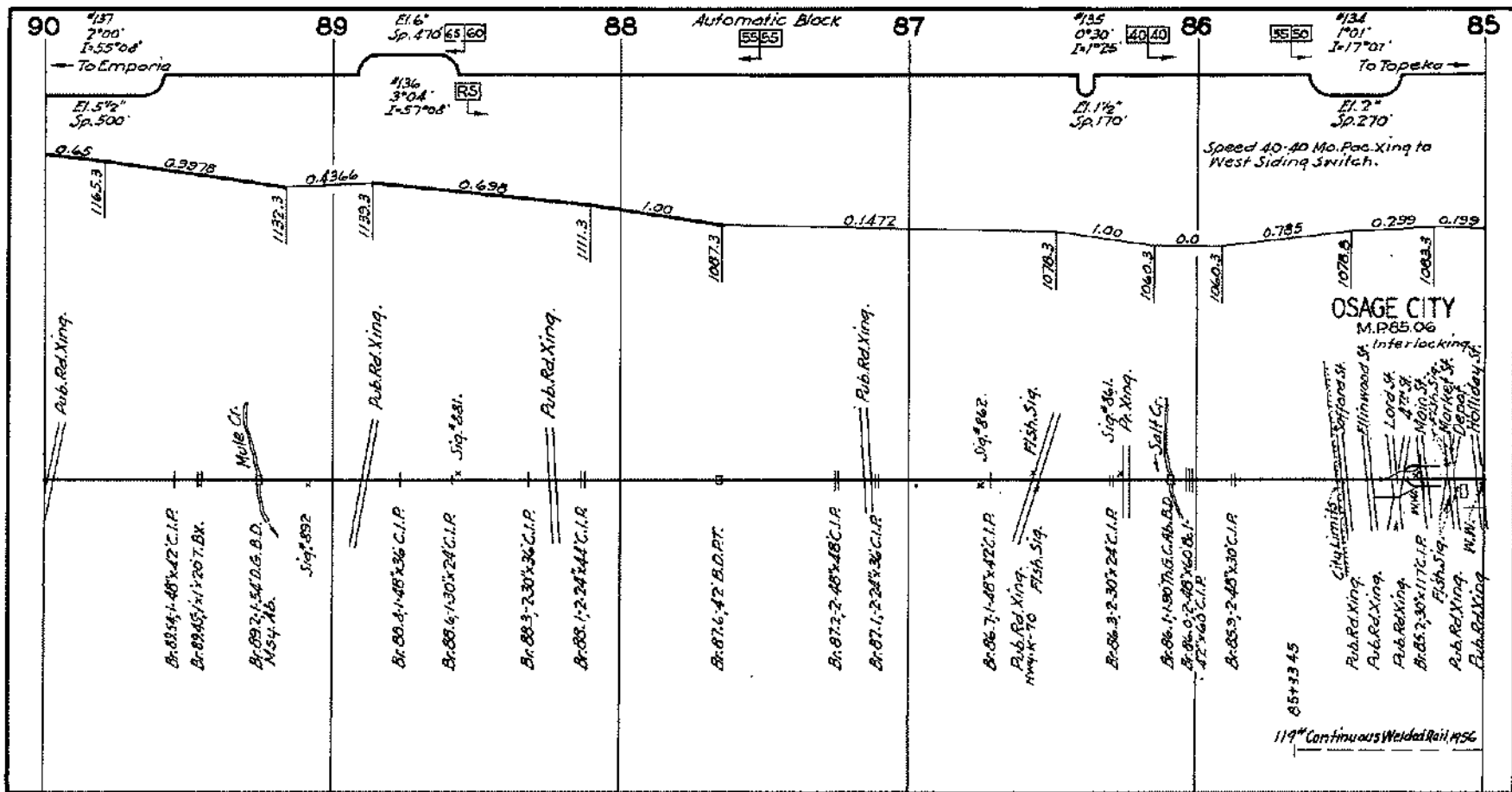
Br. 100.4; 130'x30' C.I.P.

Br. 100.3; 24'x24' C.I.P.



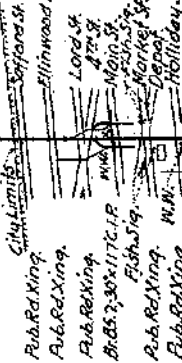
Section 10 Section 9





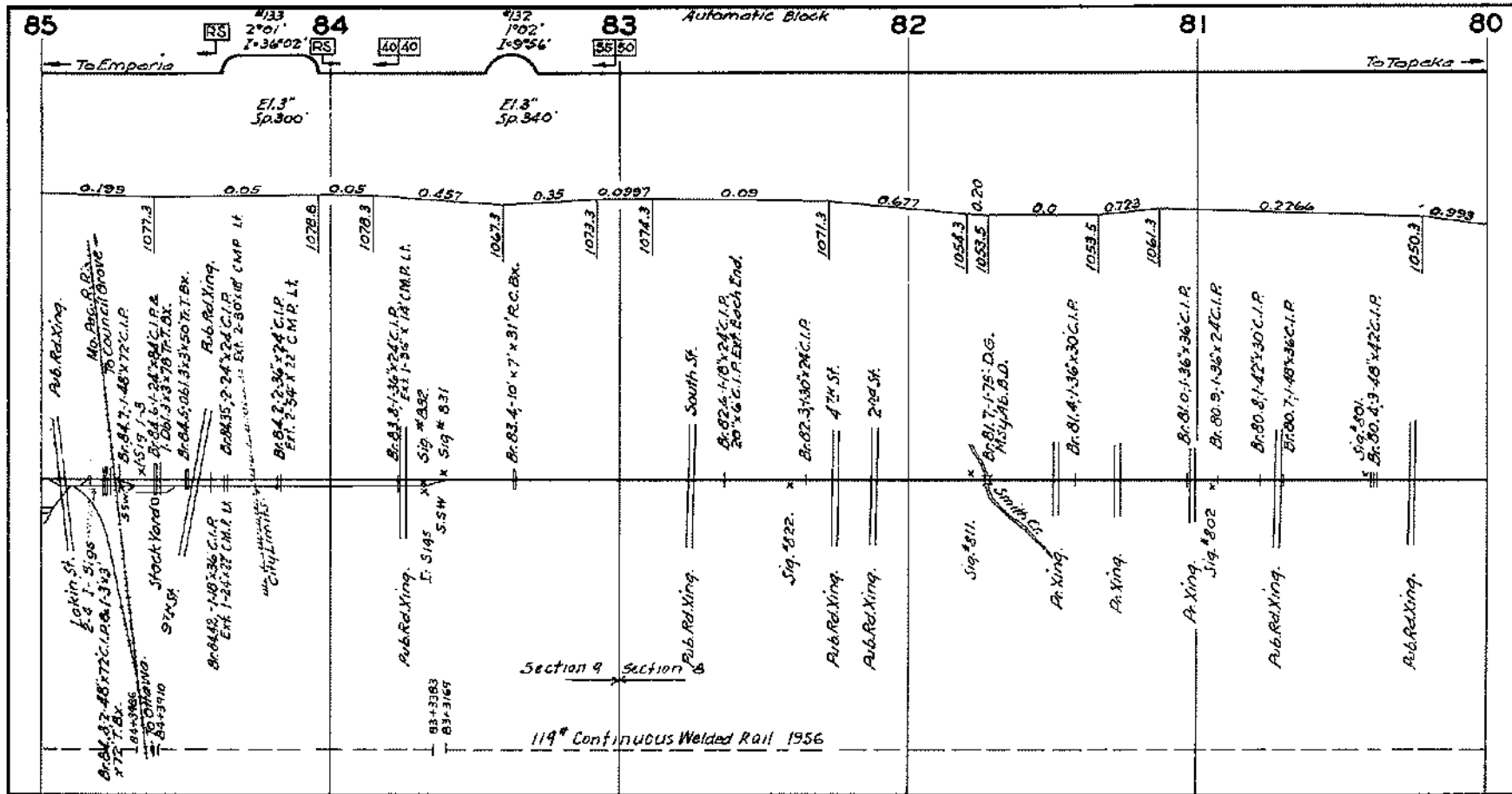
OSAGE CITY
M.P. 85.06

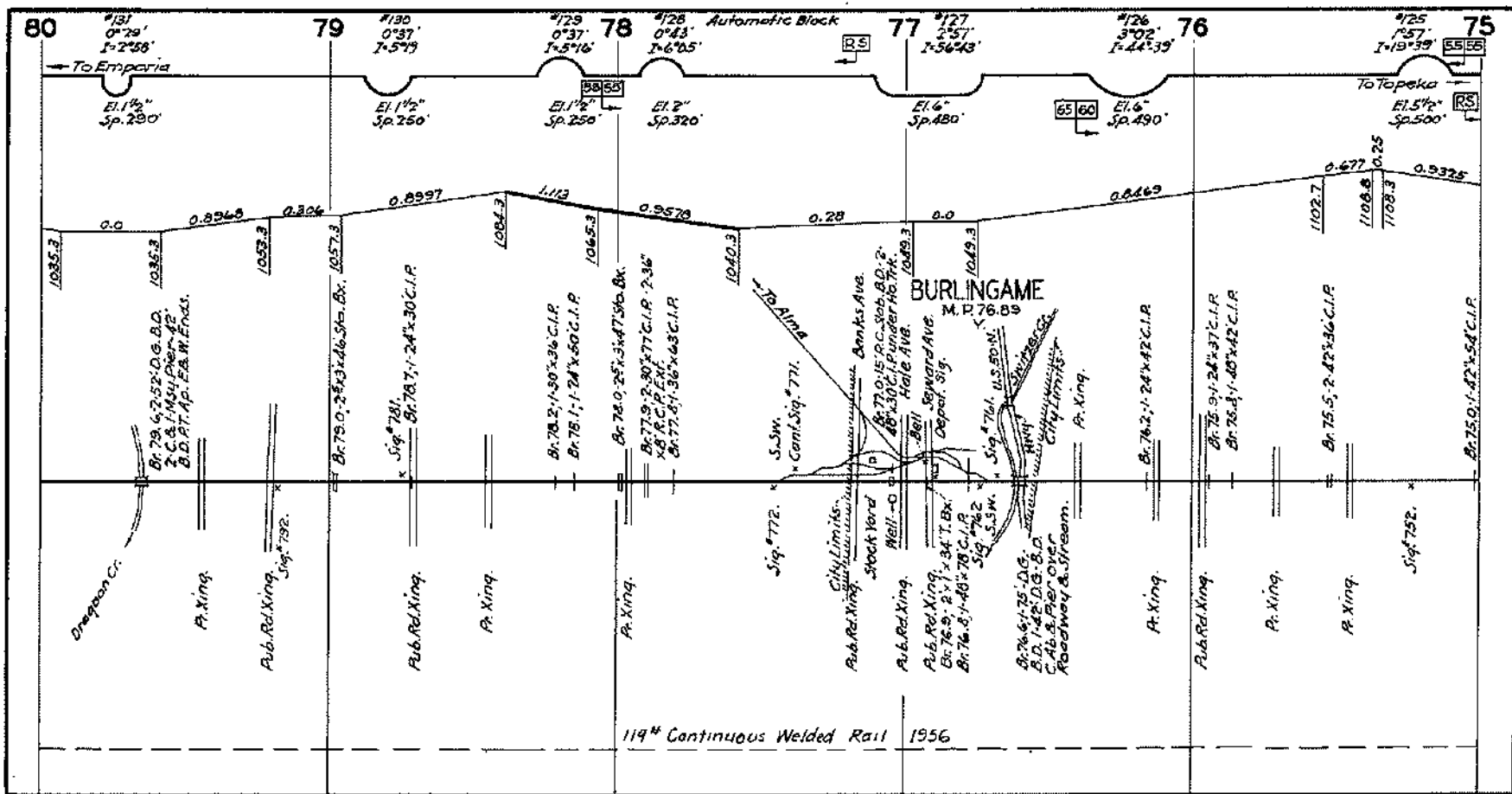
Interlocking:

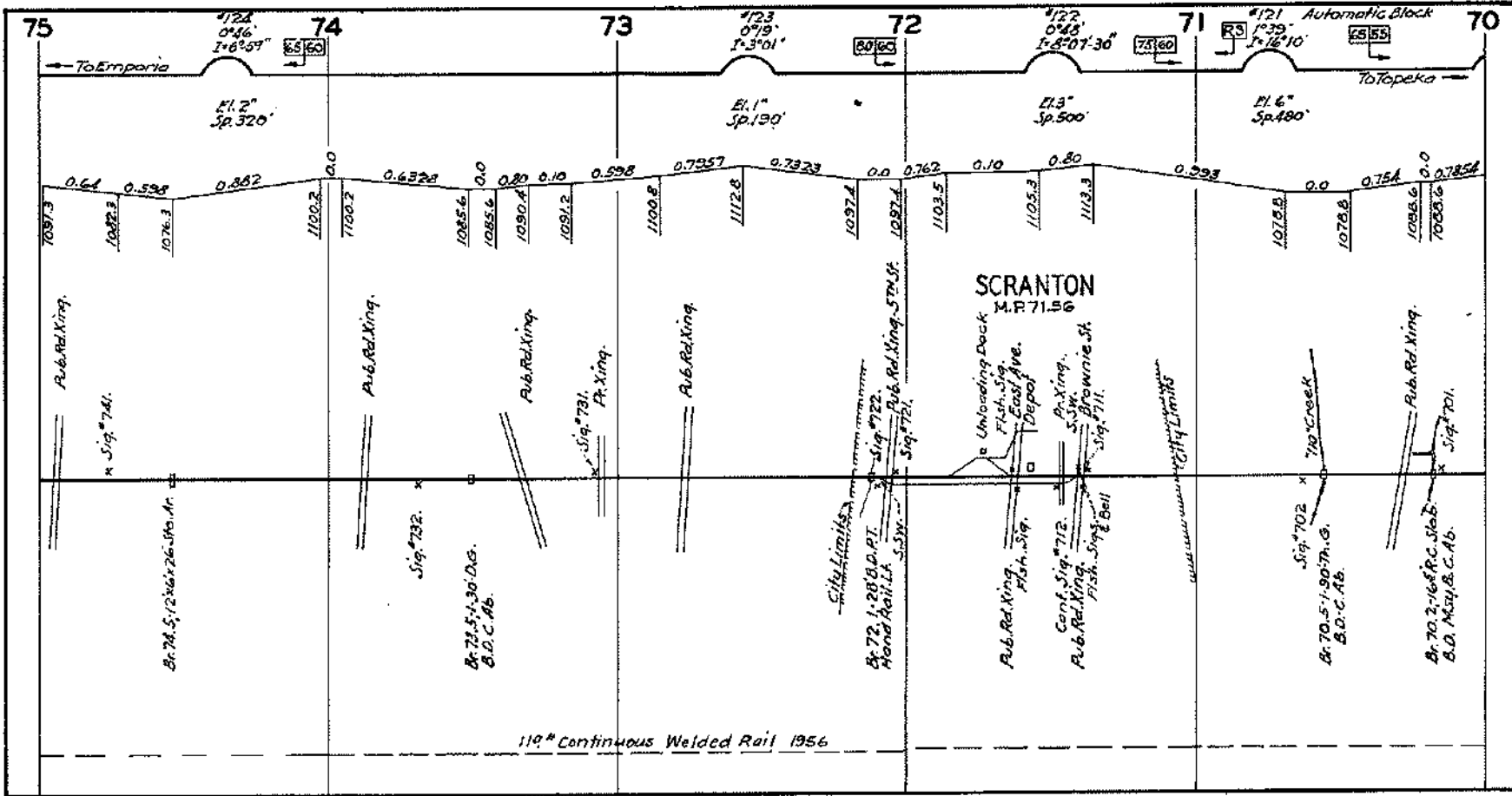


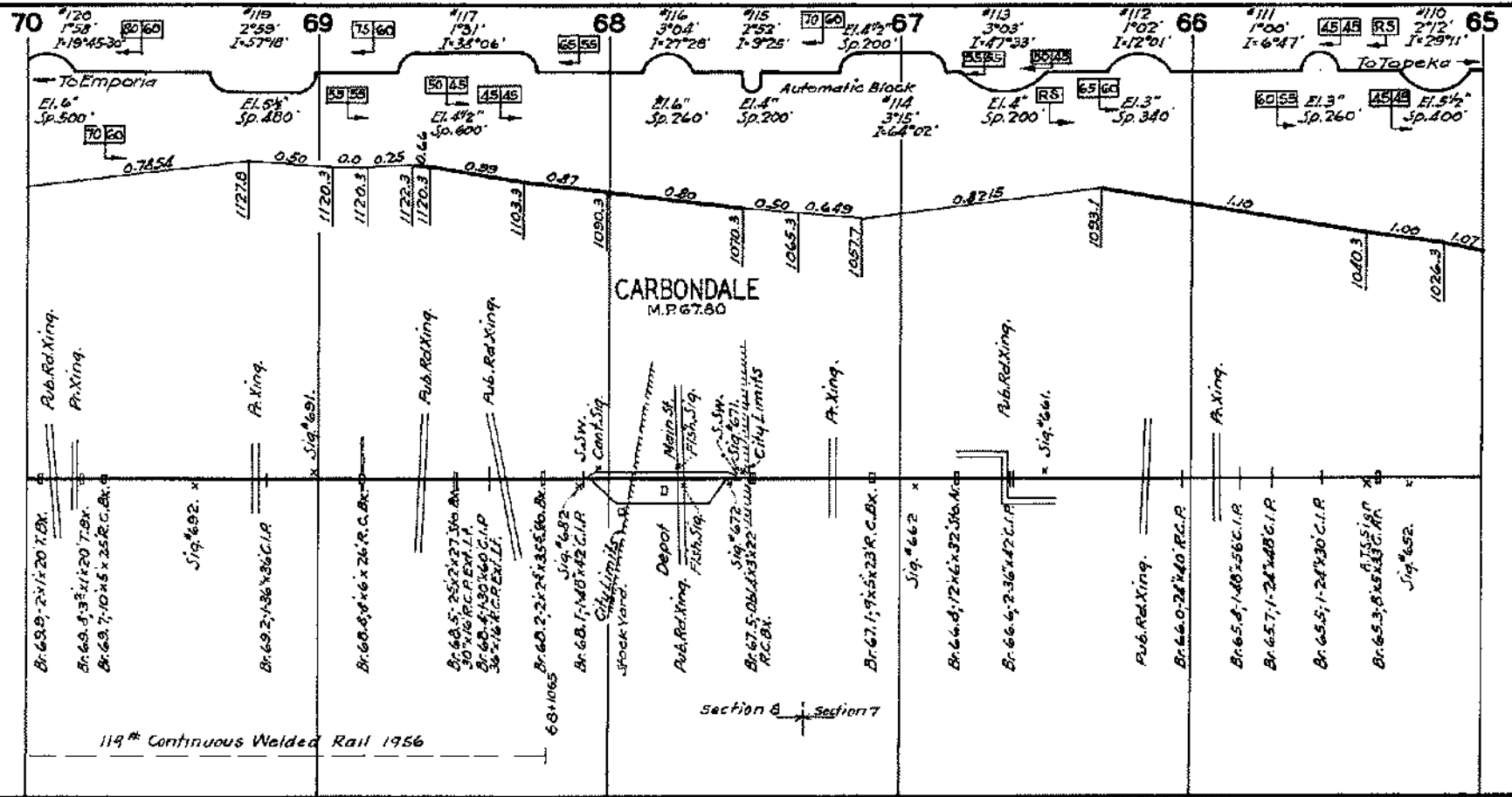
85+33.45

119" Continuous Welded Rail, 1956









Br. 69.0, 2x1x20 T.C.B.
 Br. 69.1, 3x3x120 T.C.B.
 Br. 69.2, 7-10x6x25 R.C.B.X.

Sig. #602. x
 A. Xing.
 Br. 69.2, 1/2x3/8x26 C.I.R.

Pub. Rd. Xing.
 Pub. Rd. Xing.
 Pub. Rd. Xing.
 Br. 69.3, 25x2x27 3/4 R.C.B.X.
 Br. 69.4, 30x16 R.C.B.X. L.P.
 Br. 69.5, 130x60 C.I.P.
 36x16 R.C.R. E. L.I.

Stock Yard.
 City Limits
 S.W. Conf. Sig.
 Br. 69.6, 2x2x25x35 R.C.B.X.
 Sig. #682.
 Br. 69.7, 140x42 C.I.R.

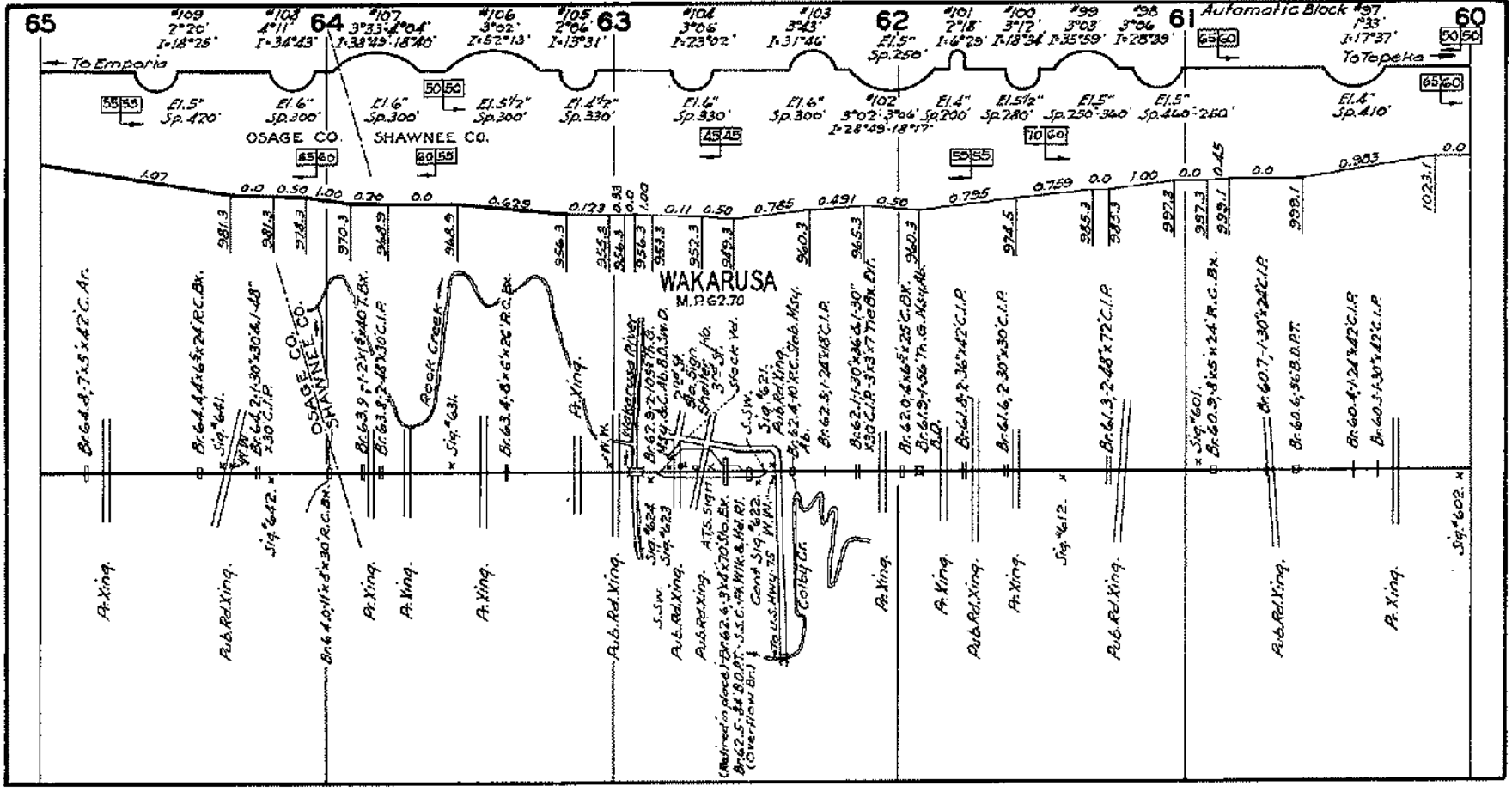
Depot
 Pub. Rd. Xing.
 Fish. Sig.
 Sig. #672.
 Br. 67.5, 0x1x3x22 L.I.P.
 R.C.B.X.
 City Limits

A. Xing.
 Br. 67.1, 19x5x23 R.C.B.X.

Pub. Rd. Xing.
 Sig. #662 x
 Br. 66.4, 12x6x32 S.Ho. Arc.
 Br. 66.4, 2x36x42 C.I.P.
 Sig. #661.

Pub. Rd. Xing.
 Br. 66.0, 2x4x40 R.C.R.

A. Xing.
 Br. 65.4, 140x56 C.I.R.
 Br. 65.7, 1-24x48 C.I.P.
 Br. 65.5, 1-24x30 C.I.P.
 Br. 65.3, 8x15x3 C.I.R.
 Sig. #652.



65

#109
2° 20'
1-18° 28'

#108
4° 11'
1-34° 43'

64
#107
3° 33' 4" 0.4'
1-38° 49' 18° 40'

#106
3° 02'
1-52° 13'

63
#105
2° 06'
1-13° 31'

#104
3° 06'
1-23° 02'

#103
3° 43'
1-31° 46'

62
#101
2° 18'
1-16° 28'

#100
3° 12'
1-18° 34'

#99
3° 03'
1-35° 59'

#98
3° 06'
1-28° 39'

61
#97
3° 17' 37"

Automatic Block #97
Totopetka
#96
3° 17' 37"
Sp. 410

60

55 55
El. 5"
Sp. 470'

50 50
El. 6"
Sp. 300'

50 50
El. 6"
Sp. 300'

45 45
El. 5 1/2"
Sp. 300'

45 45
El. 6"
Sp. 330'

50 55
El. 6"
Sp. 300'

70 60
El. 4"
Sp. 200'

65 60
El. 5"
Sp. 280'

65 60
El. 5"
Sp. 460-260'

1.97 0.0 0.50 1.00 0.70 0.0 0.628 0.173 0.03 1.00 0.11 0.50 0.785 0.491 0.50 0.795 0.759 0.0 1.00 0.0 0.45 0.0 0.385 1023.1

Br. 64.1, 7.15 x 42 C.A.

Br. 64.4, 4 x 6 x 24 R.C.B.R.
 x Sig. #41.
 Br. 64.2, 1-30 x 30 & 1-48
 x 30 C.I.P.

Br. 63.9, 1-2 x 16 & 10 T.B.R.
 Br. 63.8, 2-48 x 30 C.I.P.
 OSAGE CO.
 SHAWNEE CO.

Rock Creek
 x Sig. #43.
 Br. 63.4, 8 x 6 x 26 R.C.B.R.

A. Xing.
 W.M.
 Br. 62.9, 2-10 x 16
 W.L. & 4-6 B.O. J.W.D.
 200 yd.
 Br. 62.8, 2-10 x 16
 300 yd.

WAKARUSA
 M.P. 62.70
 S.W.
 Stock Yd.
 Sig. #21.
 Sub. R.L. Xing.
 Br. 62.4, 10 R.C. Jamb. Mary.
 A.B.

Br. 62.5, 1-24 x 18 C.I.P.
 Br. 62.1, 1-30 x 46 & 1-30'
 x 30 C.I.P. 3 x 3 x 7 T.B.R. D.P.

Br. 62.0, 4-6 x 6 x 25 C.B.K.
 Br. 61.9, 1-66 T.B. G. Mary.
 R.L.

Br. 61.8, 2-36 x 42 C.I.P.
 Br. 61.6, 2-30 x 30 C.I.P.

Br. 61.3, 2-48 x 72 C.I.P.
 x Sig. #61.

Br. 60.9, 8 x 5 x 24 R.C.B.R.
 Br. 60.7, 1-30 x 24 C.I.P.
 Br. 60.6, 36 B.O.P.T.

Br. 60.4, 1-24 x 42 C.I.P.
 Br. 60.3, 1-30 x 42 C.I.P.

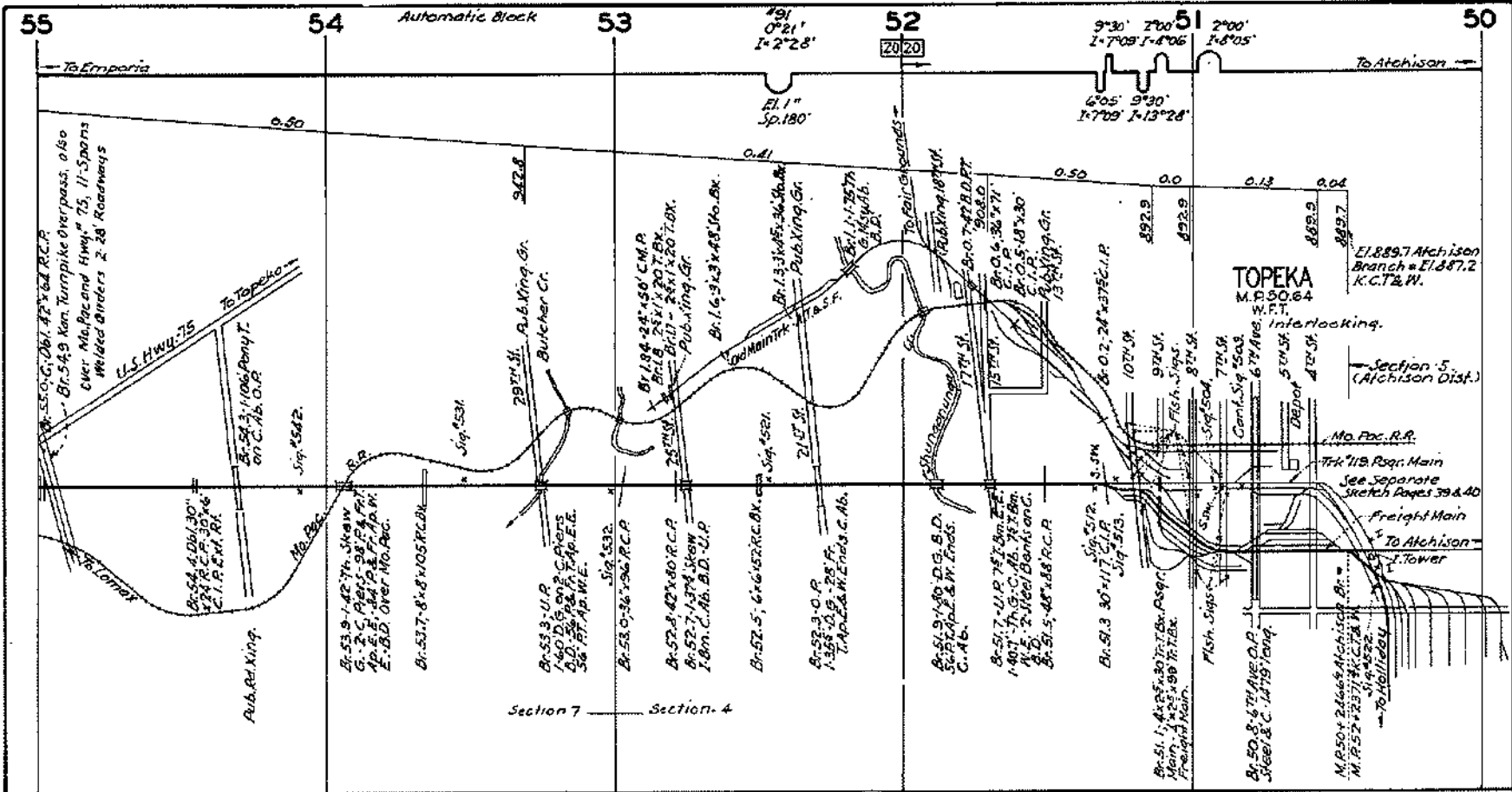
Br. 60.2, 1-30 x 42 C.I.P.
 x Sig. #62.

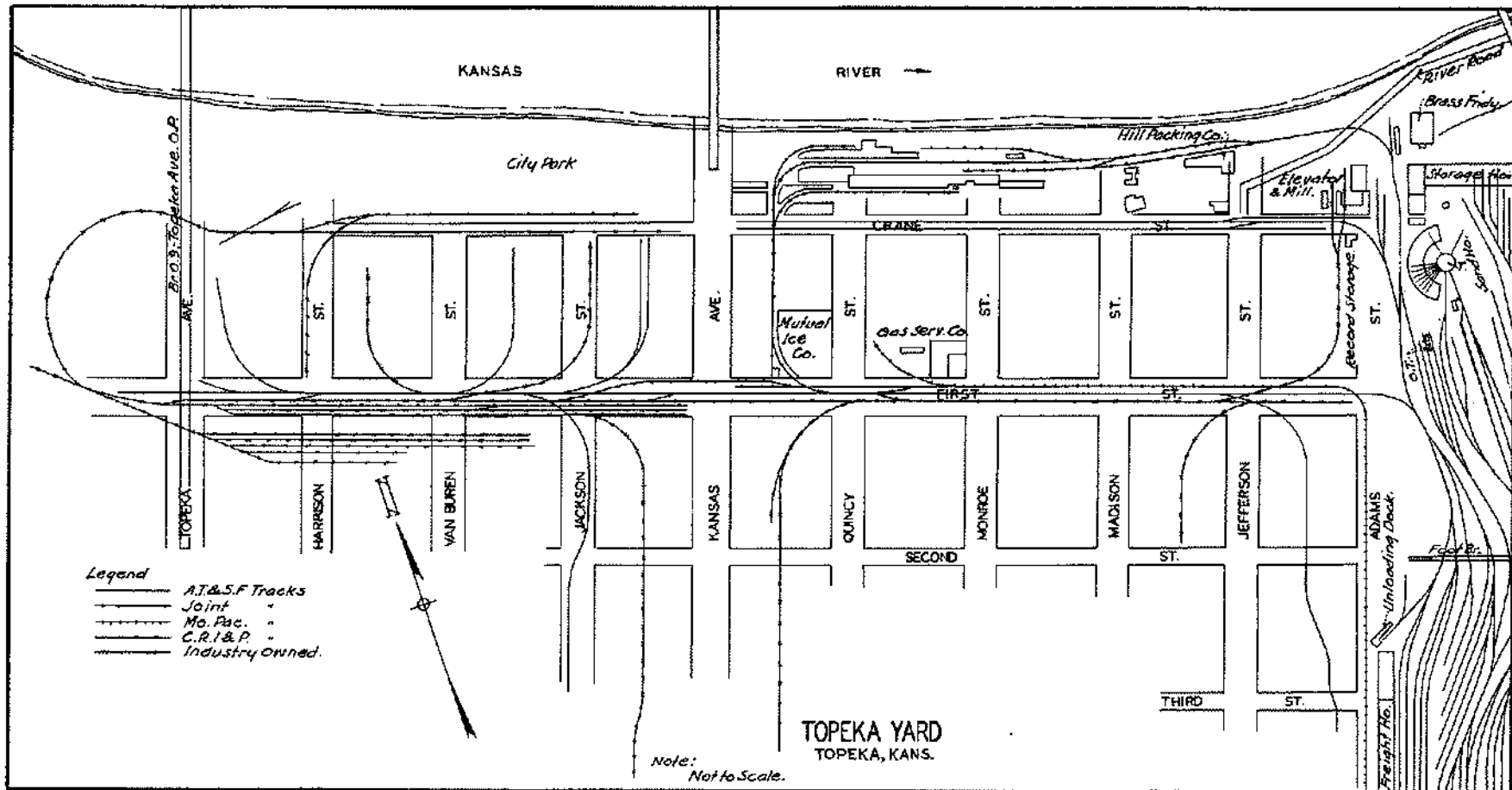
Combined in place of Br. 62.4, 3 x 4 x 70 S.W. B.R.
 Br. 62.5, 8 x 8 O.P.T. S.S.C. #4 W.L. & H.R. R.
 (Over-flow bridge)
 to U.S. Hwy. 75 W.M.

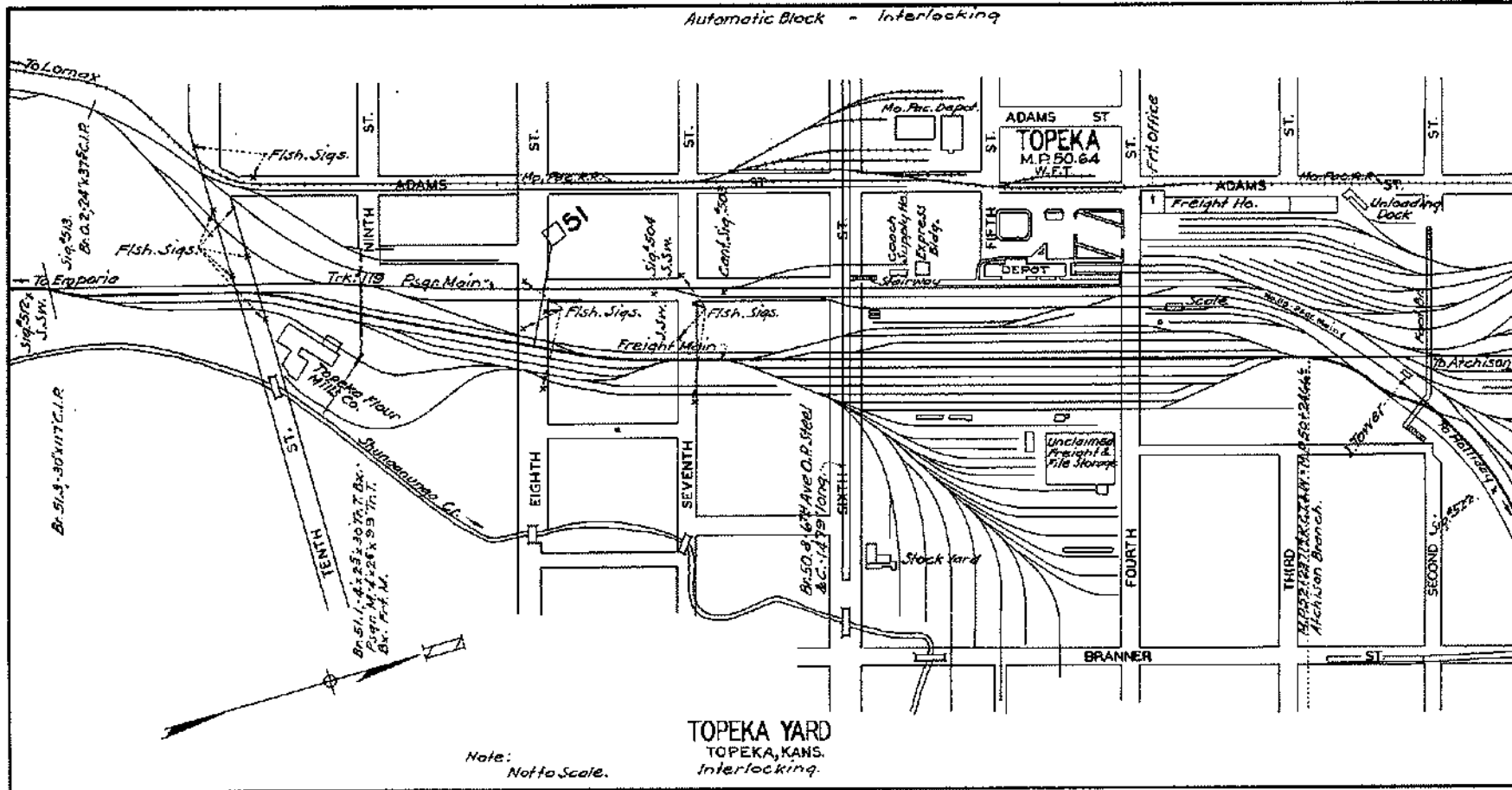
ATS. Sight

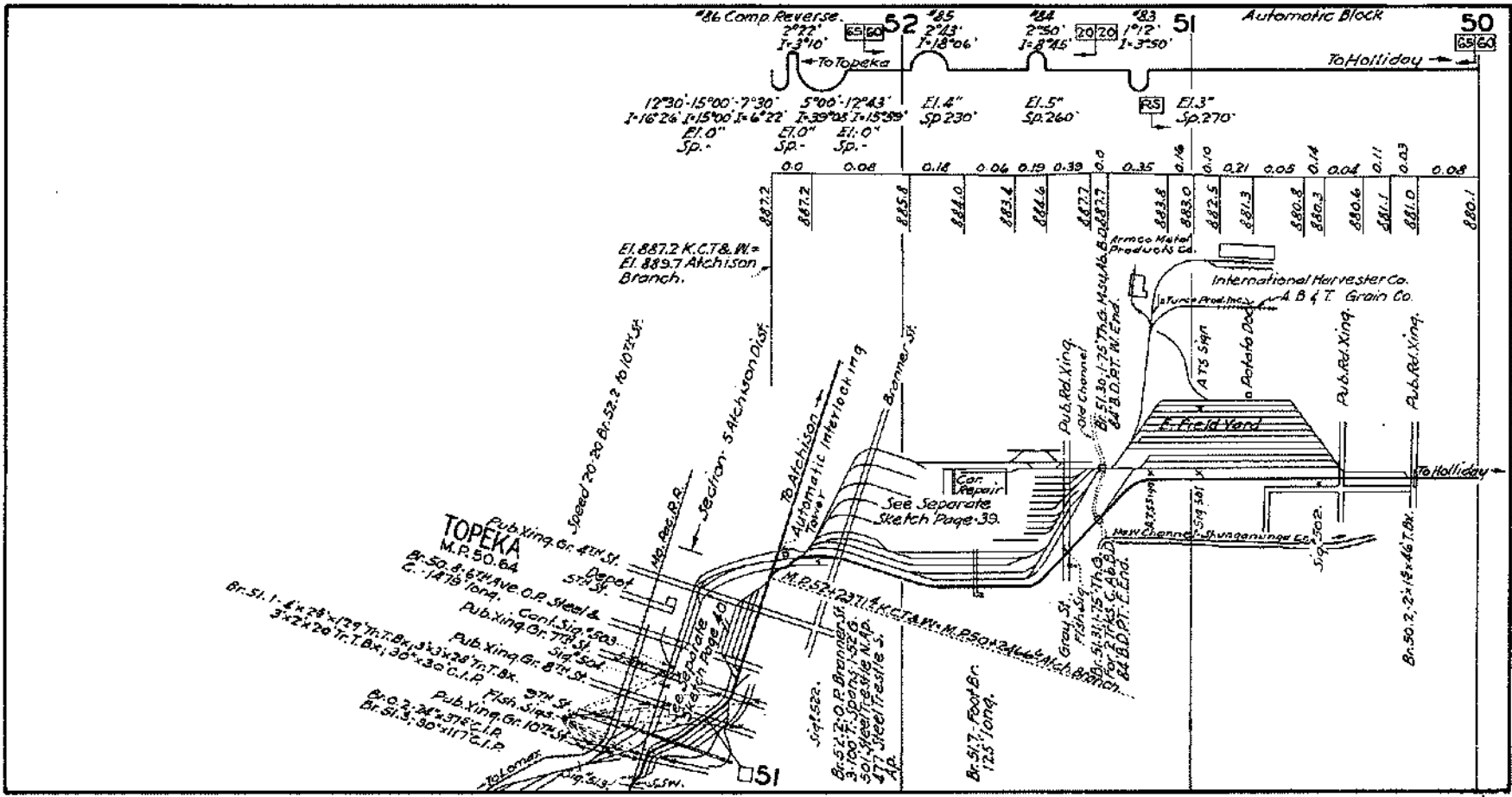
Cont. Sig. #22

Colby Cr.









86 Comp. Reverse. 52 85 84 83 Automatic Block 51 50

2°22' 1-3°10' 2°43' 1-18°06' 2°50' 1-8°45' 1°12' 1-3°50'

12°30'-15°00'-7°30' 5°00'-12°43' El. 4" El. 5" El. 3"

1-16°26' 1-15°00' 1-6°22' 1-39°05' 1-15°39' Sp 230' Sp 260' Sp 270'

El. 0" El. 0" El. 0"

Sp. Sp. Sp.

0.0	0.08	0.18	0.06	0.19	0.39	0.0	0.35	0.16	0.10	0.21	0.05	0.14	0.08	0.11	0.03	0.08
-----	------	------	------	------	------	-----	------	------	------	------	------	------	------	------	------	------

El. 887.2 K.C.T. & M.
El. 889.7 Atchison Branch.

TOPEKA
M.P. 50.64

Pub. Xing. Cr. 4th St.
Depot 5th St.
Br. 50.8, 6th Ave. O.P. Steel & C. 14' 18' long.
Pub. Xing. Cr. 7th St.
Cont. Sign 503.
Pub. Xing. Cr. 8th St.
5th St.
Fish Sigs.
Pub. Xing. Cr. 10th St.
60.2, 24' x 37' C.I.P.
Br. 51.3, 30' x 17' C.I.P.
Sigs. 513.
Sigs. 514.
Sigs. 515.

See Separate Sketch Page 33.

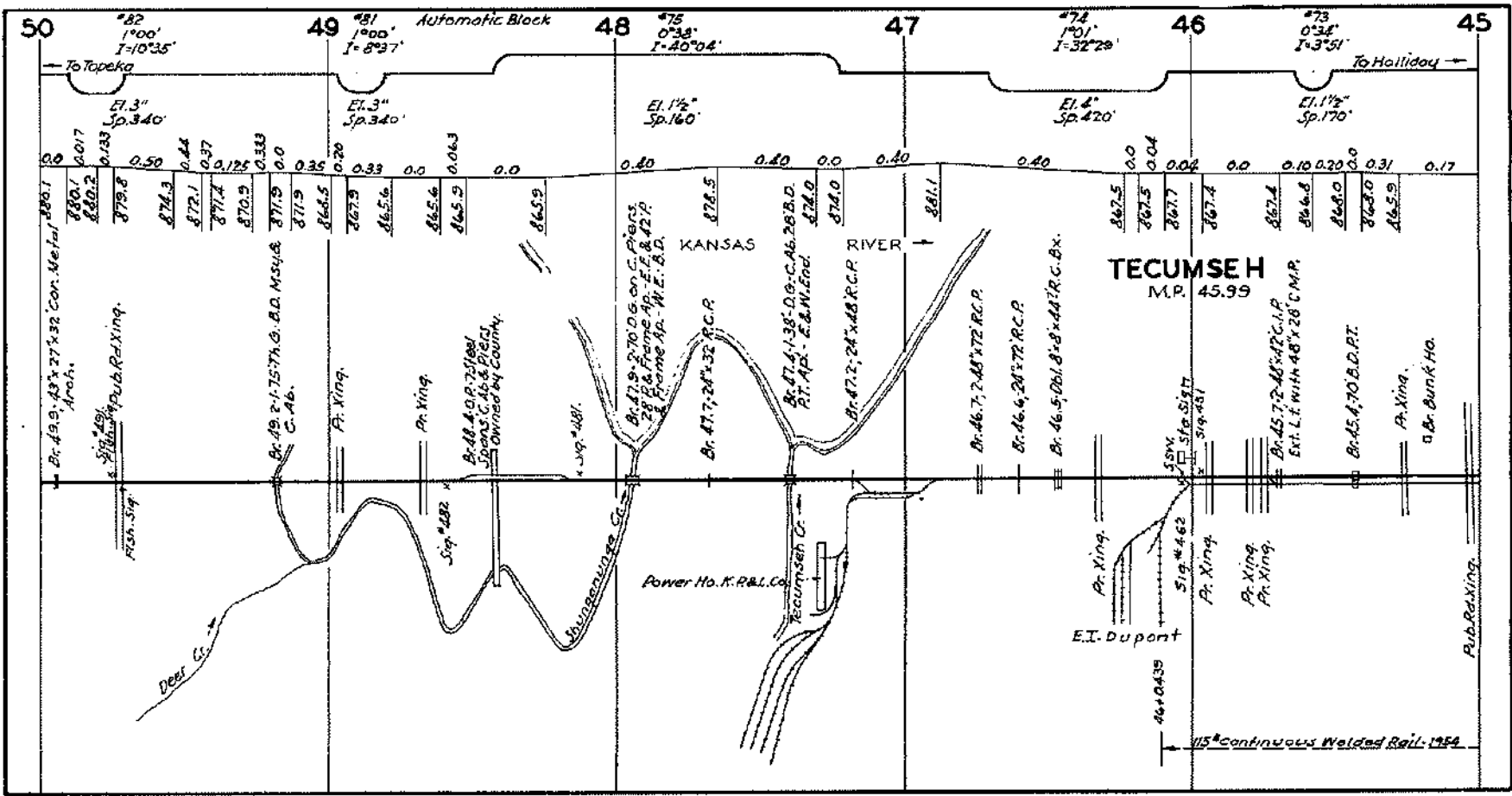
Pub. Red Xing. one Channel
Br. 51.30, 1-75' Th. G. M.S. 46. B. D. 87' W. End.
Br. 51.30, 1-75' Th. G. M.S. 46. B. D. 87' W. End.
Br. 51.30, 1-75' Th. G. M.S. 46. B. D. 87' W. End.

International Harvester Co.
A B & T Grain Co.
ATS Sign
Potato Dock
Pub. Red Xing.
Pub. Red Xing.
To Holiday

Br. 51.7, Foot Br. 125' long.

Speed 70-20 Br. 52.2 to 10 24' Sp.
Section 5 Atchison Dist.
Automatic Interlock Trg.
Boomer, Jr.

Br. 52.7, O.P. Brimmer St. 3' 100' 7' Spans, 1-32' G. 50' x 12' x 12' N.P. 477 Steel Truss 51.
Br. 51.7, 51.75 Th. G. 84' B.D.P.T. L. End.



50

49

48

47

46

45

Automatic Block

To Topeka

To Holliday

E1.3" Sp.340'

E1.3" Sp.340'

E1.1 1/2" Sp.160'

E1.4" Sp.420'

E1.1 1/2" Sp.170'

0.0 0.017 0.133 0.50 0.44 0.37 0.125 0.333 0.0 0.35 0.20 0.33 0.0 0.063 0.0 0.0 0.40 0.40 0.0 0.40 0.0 0.04 0.04 0.02 0.0 0.10 0.20 0.0 0.31 0.17

880.1 880.1 880.2 879.8 874.3 872.1 871.4 870.9 871.9 871.9 866.5 867.9 865.6 865.6 865.9 865.9 878.5 878.5 874.0 874.0 881.1 867.5 867.5 867.7 867.4 867.4 866.8 868.0 868.0 865.9

KANSAS RIVER

TECUMSEH M.P. 45.99

Br. 49.9, 43' x 27' x 32' Cor. Metal Arch.
 Sig. #491
 Fish. Sig.
 Pub. R.R. Xing.

Br. 49.2, 1-15-75 Th. G. B.D. M.S. 48
 C. A. 6.

Pr. Xing.

Pr. Xing.

Sig. #482
 Br. 48.4, 0-9-75 1081
 J. O. S. C. A. 6. 8' 10 1/2'
 Owned by County.

Sig. #481

Br. 47.9, 2-70-0-6 on C. P. 1000
 Br. 47.8, 1-38-0-9 on C. P. 1000
 Br. 47.7, 1-38-0-9 on C. P. 1000
 Br. 47.6, 1-38-0-9 on C. P. 1000
 Br. 47.5, 1-38-0-9 on C. P. 1000
 Br. 47.4, 1-38-0-9 on C. P. 1000
 Br. 47.3, 1-38-0-9 on C. P. 1000
 Br. 47.2, 1-38-0-9 on C. P. 1000
 Br. 47.1, 1-38-0-9 on C. P. 1000
 Br. 47.0, 1-38-0-9 on C. P. 1000
 Br. 46.9, 1-38-0-9 on C. P. 1000
 Br. 46.8, 1-38-0-9 on C. P. 1000
 Br. 46.7, 1-38-0-9 on C. P. 1000
 Br. 46.6, 1-38-0-9 on C. P. 1000
 Br. 46.5, 1-38-0-9 on C. P. 1000
 Br. 46.4, 1-38-0-9 on C. P. 1000
 Br. 46.3, 1-38-0-9 on C. P. 1000
 Br. 46.2, 1-38-0-9 on C. P. 1000
 Br. 46.1, 1-38-0-9 on C. P. 1000
 Br. 46.0, 1-38-0-9 on C. P. 1000
 Br. 45.9, 1-38-0-9 on C. P. 1000
 Br. 45.8, 1-38-0-9 on C. P. 1000
 Br. 45.7, 1-38-0-9 on C. P. 1000
 Br. 45.6, 1-38-0-9 on C. P. 1000
 Br. 45.5, 1-38-0-9 on C. P. 1000
 Br. 45.4, 1-38-0-9 on C. P. 1000
 Br. 45.3, 1-38-0-9 on C. P. 1000
 Br. 45.2, 1-38-0-9 on C. P. 1000
 Br. 45.1, 1-38-0-9 on C. P. 1000
 Br. 45.0, 1-38-0-9 on C. P. 1000

Johnsonville Cr.

Power Ho. K. R. & L. Co.

Tecumseh Cr.

Br. 46.7, 2-48' x 72' R.C.P.

Br. 46.4, 20' x 72' R.C.P.

Br. 46.5, 0-61.8' x 44' R.C. Bx.

E.I. Dupont
 Pr. Xing.
 Sig. #462
 46+0439

Sig. #461

Pr. Xing.

Pr. Xing.

Br. 45.7, 2-48' x 72' R.C.P.

Ext. L. with 48' x 28' C.M.P.

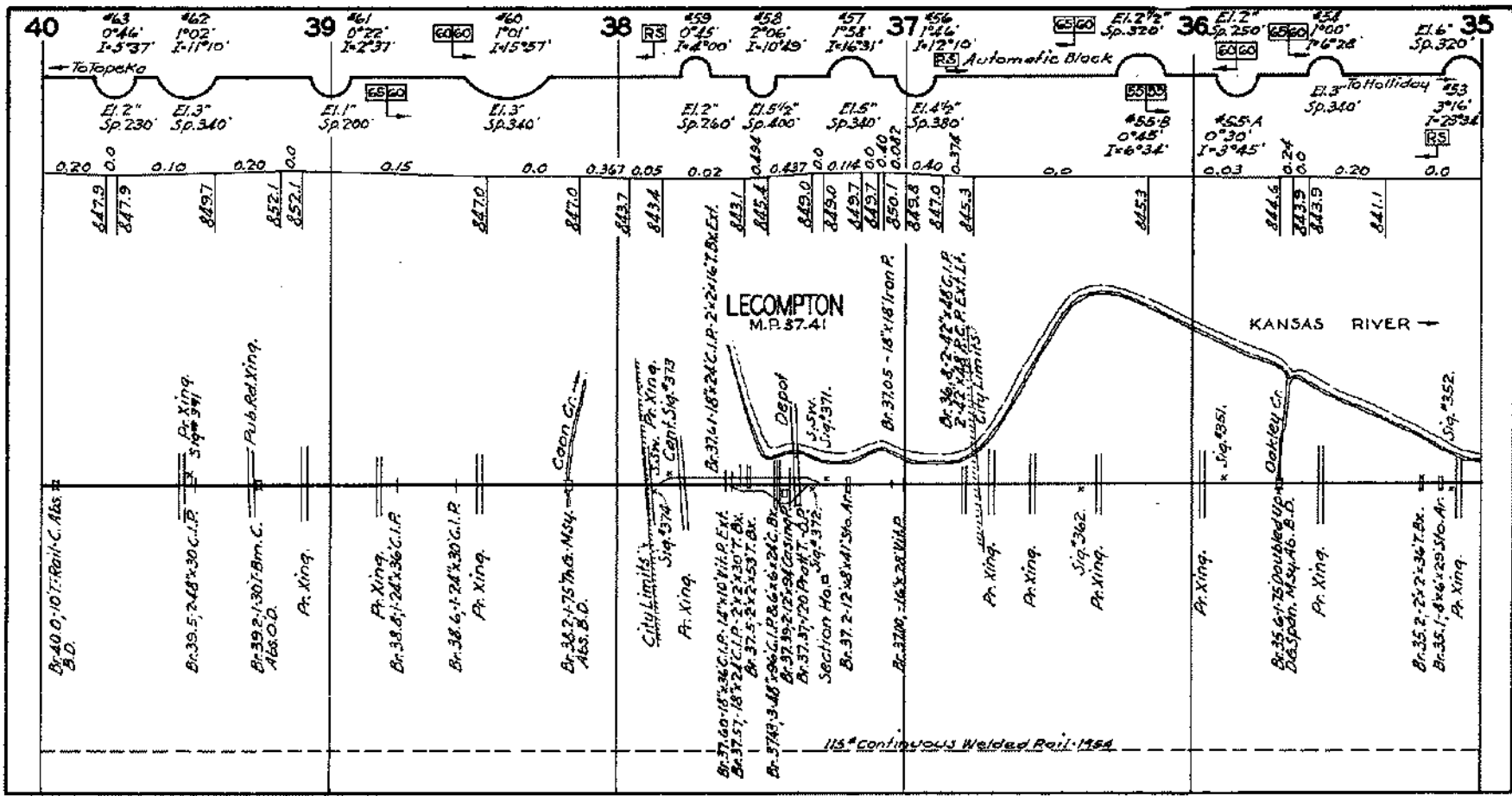
Br. 45.4, 70' B.D.P.T.

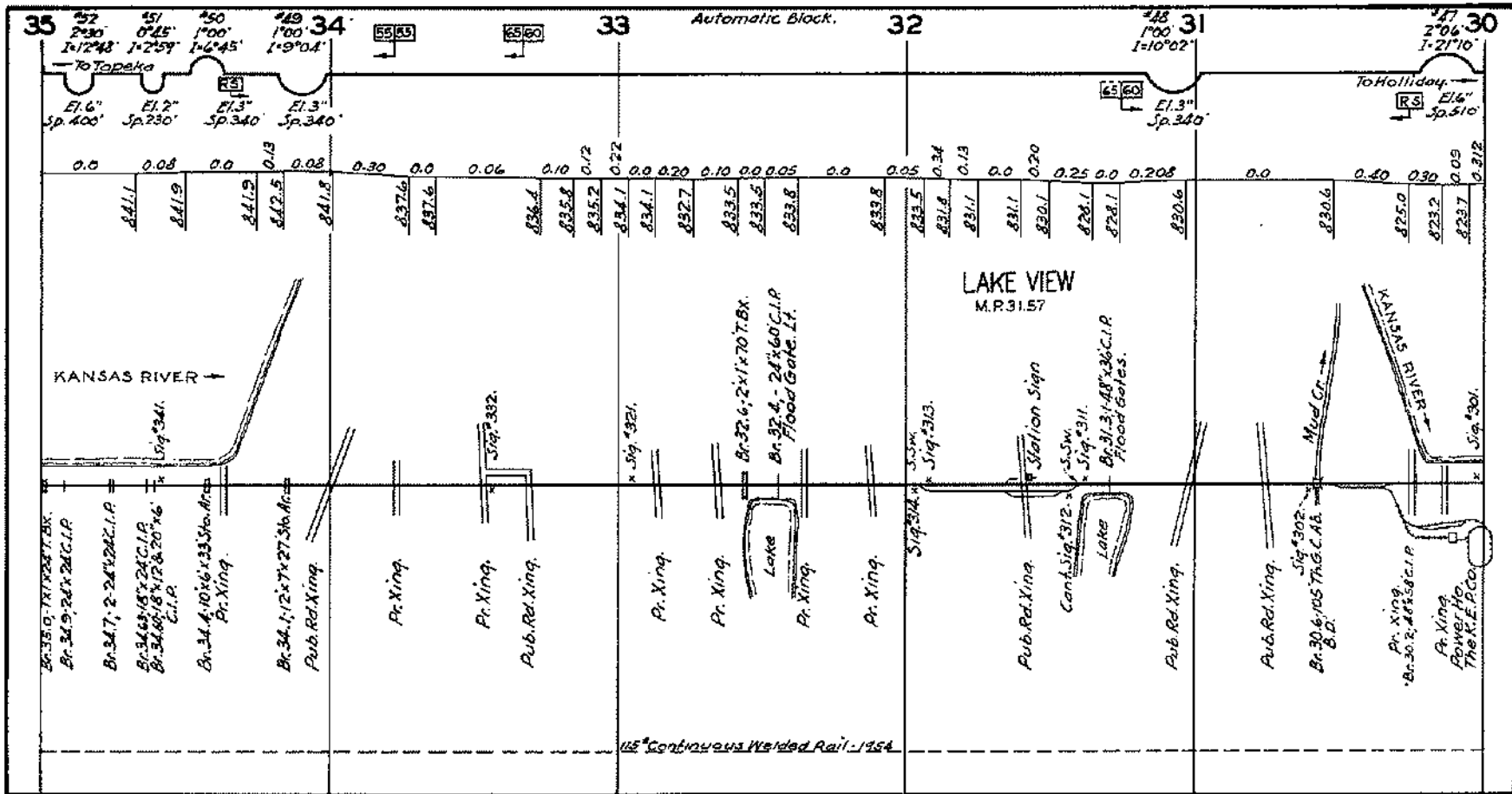
Pr. Xing.

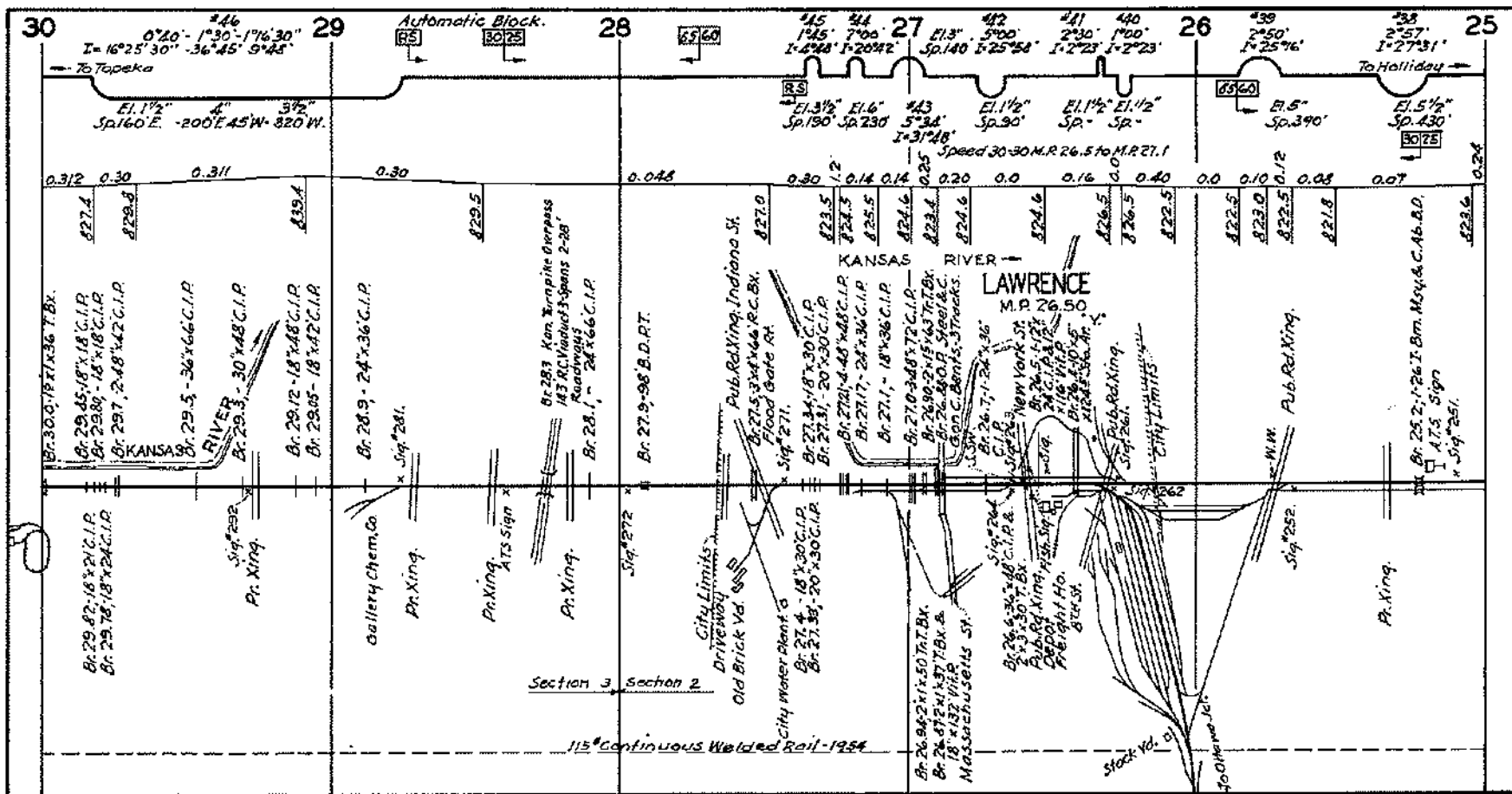
Br. Bunk Ho.

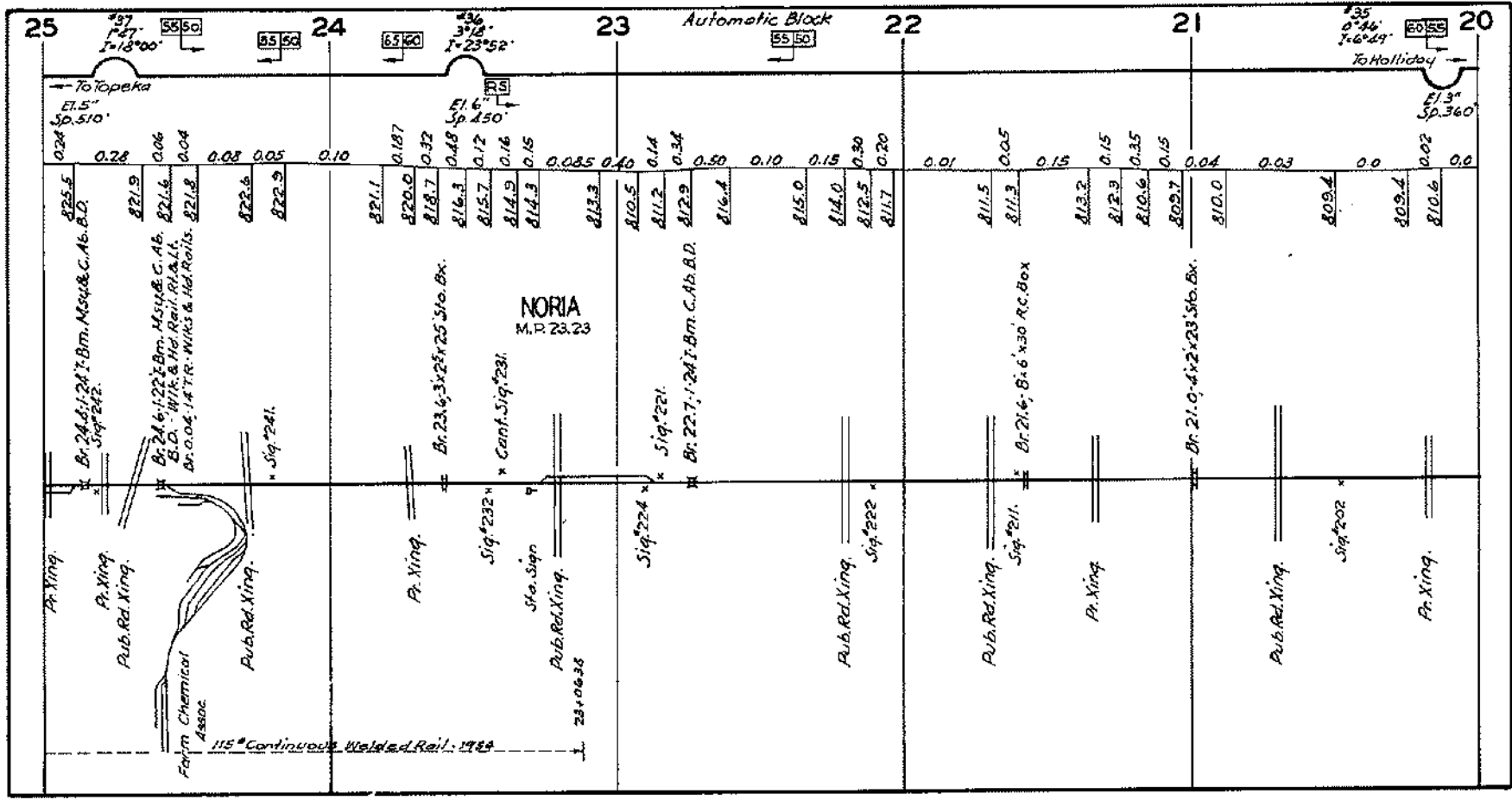
Pub. R.R. Xing.

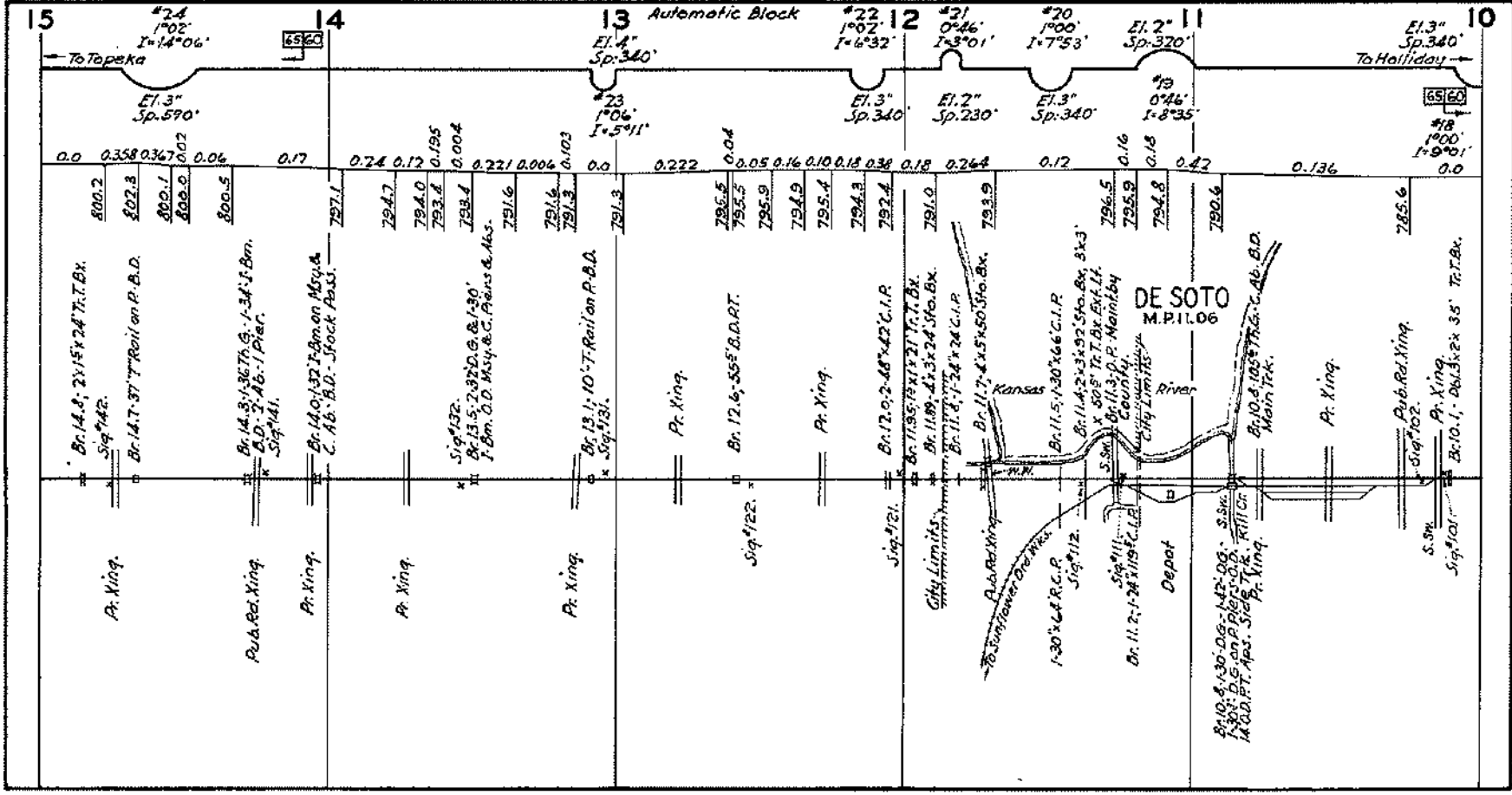
115' Continuous Welded Rail - 1954



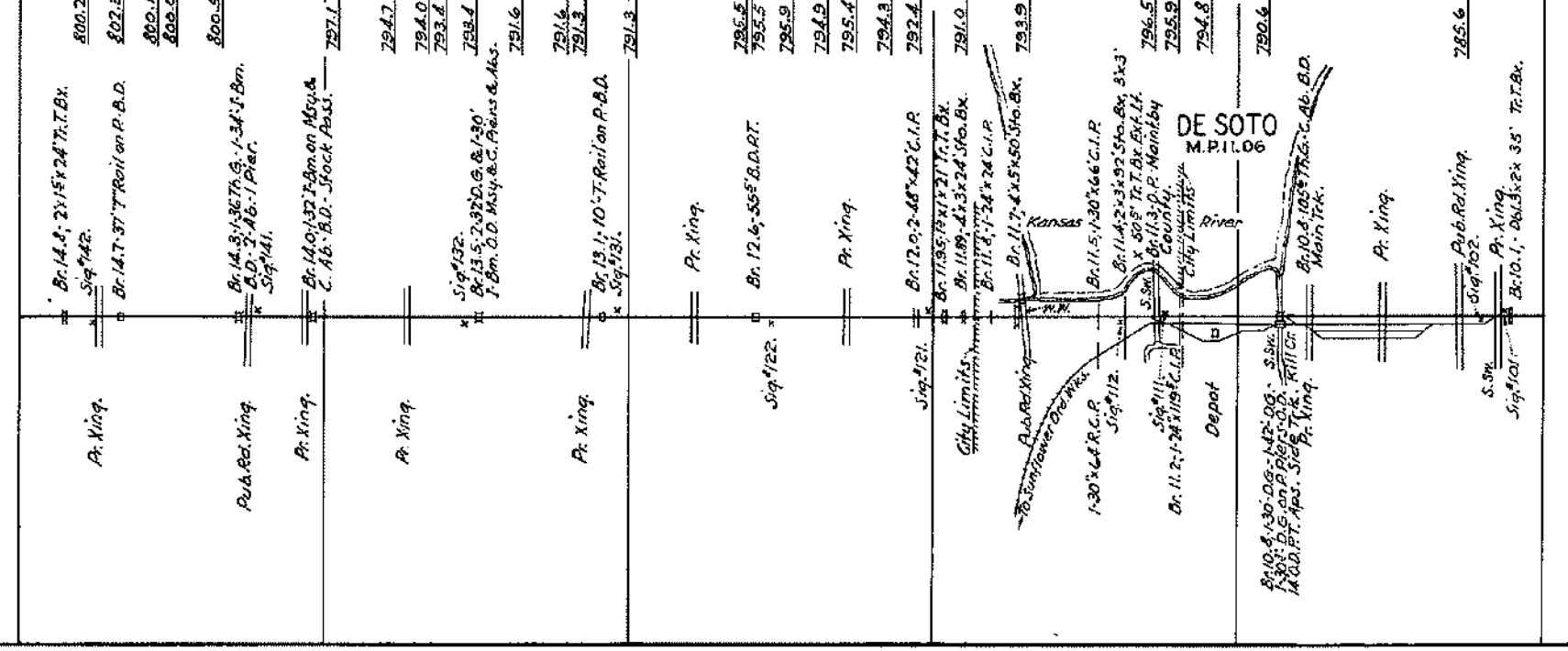


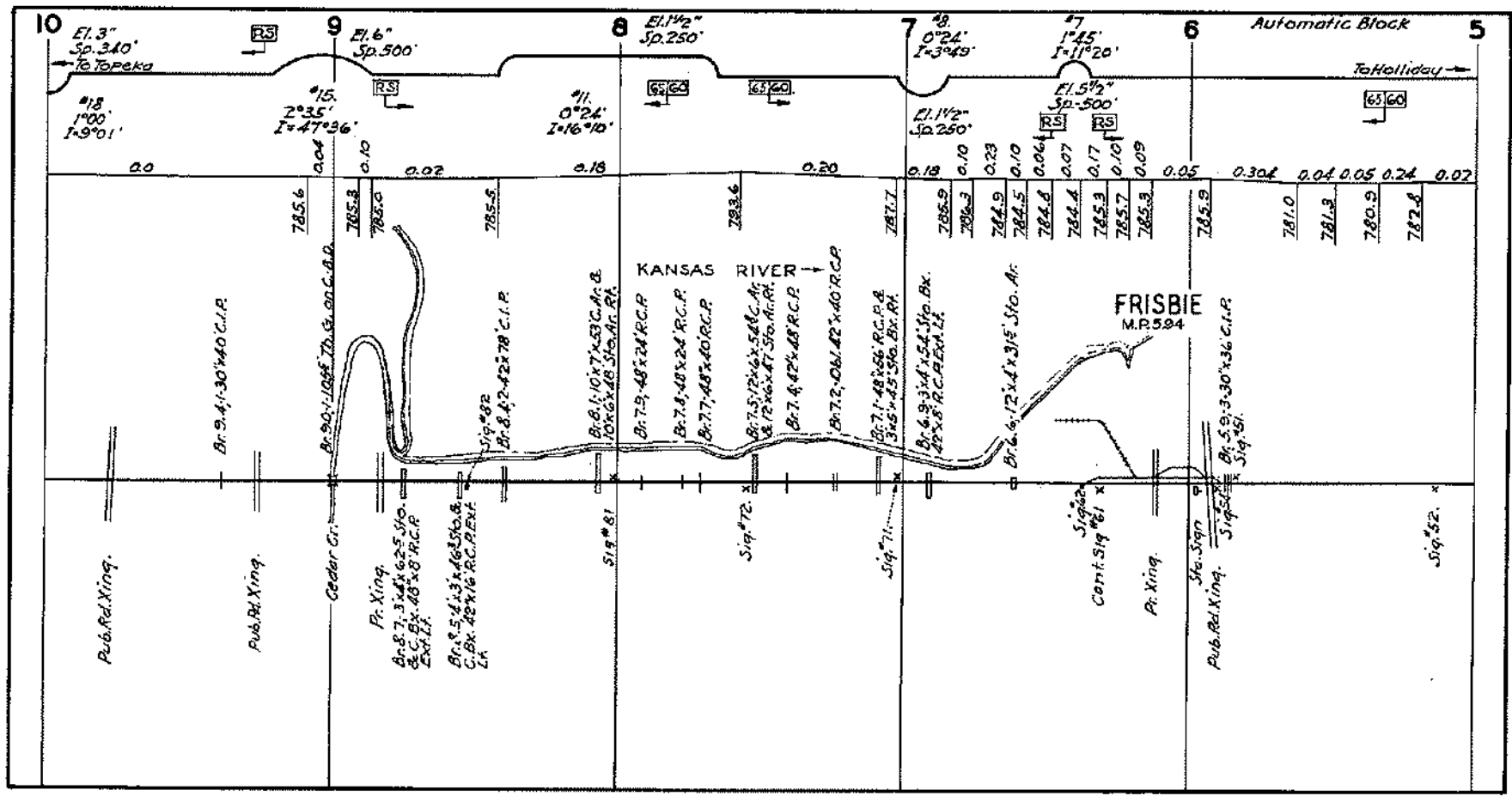






Section	Station	Elevation	Spot
15	0.0	El. 1.3"	Sp. 570'
	0.358	El. 1.4°06'	
	0.367		
	0.06		
	0.17		
14	0.74		
	0.12		
	0.195		
	0.004		
	0.221		
13 Automatic Block	0.004		
	0.103		
	0.0	El. 1.4"	Sp. 340'
	0.222	El. 2.3"	Sp. 340'
	0.04		
12	0.05		
	0.16		
	0.10		
	0.18		
	0.38		
21	0.18	El. 1.2"	Sp. 230'
	0.264	El. 1.3"	Sp. 340'
	0.12		
	0.16		
	0.18	El. 2.1"	Sp. 370'
20	0.42		
	0.16		
	0.18		
	0.42		
	0.136		
11	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
10	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'
	0.0	El. 1.3"	Sp. 340'





10 El. 3.3" Sp. 3.10' To Topeka

9 El. 6" Sp. 500'

8 El. 142" Sp. 250'

7 El. 112" Sp. 250'

6 Automatic Block

5 To Holliday

#15 1"00' I=9"01'

#11 0"24' I=16"10'

#48 0"24' I=3"49'

#47 1"45' I=11"20'

#15 2"35' I=47"36'

0.00 0.04 0.10 0.02 0.18 0.20 0.18 0.10 0.23 0.10 0.06 0.07 0.17 0.10 0.09 0.05 0.30d 0.04 0.05 0.24 0.07

785.6 785.3 785.0 785.5 783.6 787.7 788.9 789.3 784.9 784.5 784.8 784.4 785.3 785.7 785.3 785.9 781.0 781.3 780.9 787.8

Br. 9.4:1-30'x40' C.I.P.

Br. 9.1:106' Th. 9. on C.B.D.

Br. 5.4:1-3'x46' Sto. B. C. Bx. 48"x8" R.C.P. EX. L.I.

Br. 5.4:1-3'x46' Sto. B. C. Bx. 42"x16" R.C.P. EX. L.I.

Br. 1.10:1-7'x53' C. A. B. 108"x48" Sto. A. R.P.

Br. 7.9:48'x24' R.C.P.

Br. 7.4:48'x24' R.C.P.

Br. 7.7:48'x40' R.C.P.

Br. 7.5:72"x6'x54" C. A. B. 12"x6"x47' Sto. A. R.P.

Br. 7.4:42'x48' R.C.P.

Br. 7.2:106'x42'x40' R.C.P.

Br. 7.1:48'x54' R.C.P. B. 3'x5'x45' Sto. Bx. R.P.

Br. 6.9:34'x54' Sto. Bx. 42"x8" R.C.P. EX. L.I.

Br. 6.6:12'x4'x3'5" Sto. A. C.

FRISBIE M.R. 5594

Br. 5.9:3'30'x36' C.I.P.

Pub. Rd. Xing.

Pub. Rd. Xing.

Cedar Cr.

P. Xing.

Sig. #81

Sig. #72

Sig. #71

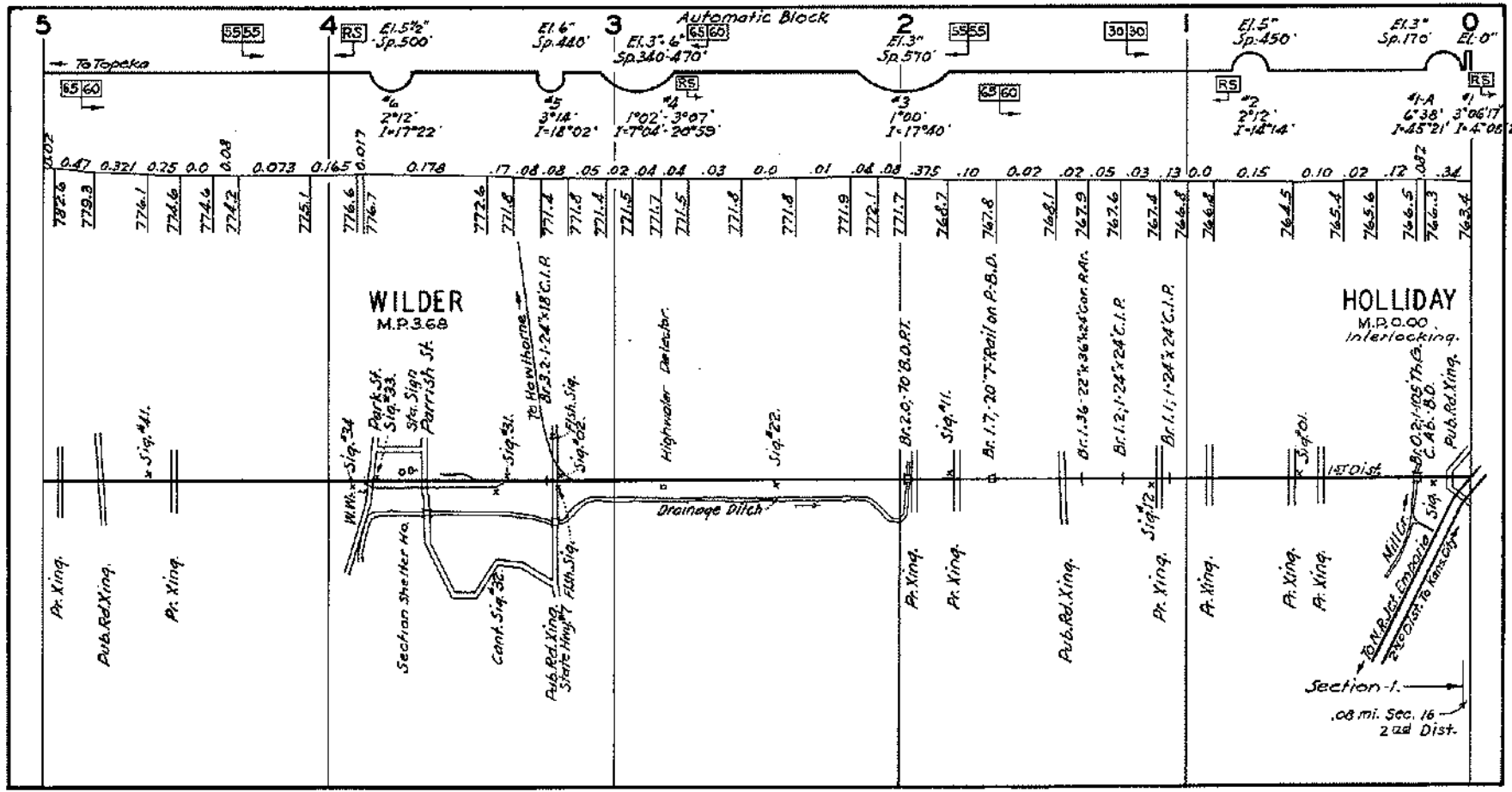
Cont. Sig. #61

A. Xing.

Stn. Sign

Pub. Rd. Xing.

Sig. #52



Station	Grade	Sp.	Angle	Dist.
5	El. 5.5'	Sp. 500'		
4	El. 5.5'	Sp. 500'		
3	El. 6'	Sp. 440'		
3	El. 3.6'	Sp. 340'-470'		
2	El. 3.3'	Sp. 570'		
1	El. 5.5'	Sp. 450'		
0	El. 3.0'	Sp. 170'		

Station	Grade	Sp.	Angle	Dist.
782.6	0.47	0.321	0.25	0.0
779.3	0.073	0.165	0.017	
776.1	0.178			
774.6	0.178			
774.6	0.05	0.02	0.04	0.04
774.2	0.03	0.0	0.01	0.08
775.1	0.375	0.10	0.02	0.05
776.6	0.07	0.03	0.13	0.0
776.7	0.15	0.10	0.07	0.12
772.6	0.082			
771.8	0.34			
771.4				
771.8				
771.4				
771.6				
771.7				
771.5				
771.8				
771.8				
771.9				
772.1				
771.7				
766.7				
767.8				
766.1				
767.9				
767.6				
767.4				
766.8				
766.4				
764.5				
765.4				
765.6				
766.5				
766.3				
763.4				

WILDER
M.P. 368

HOLLIDAY
M.P. 00
Interlocking.

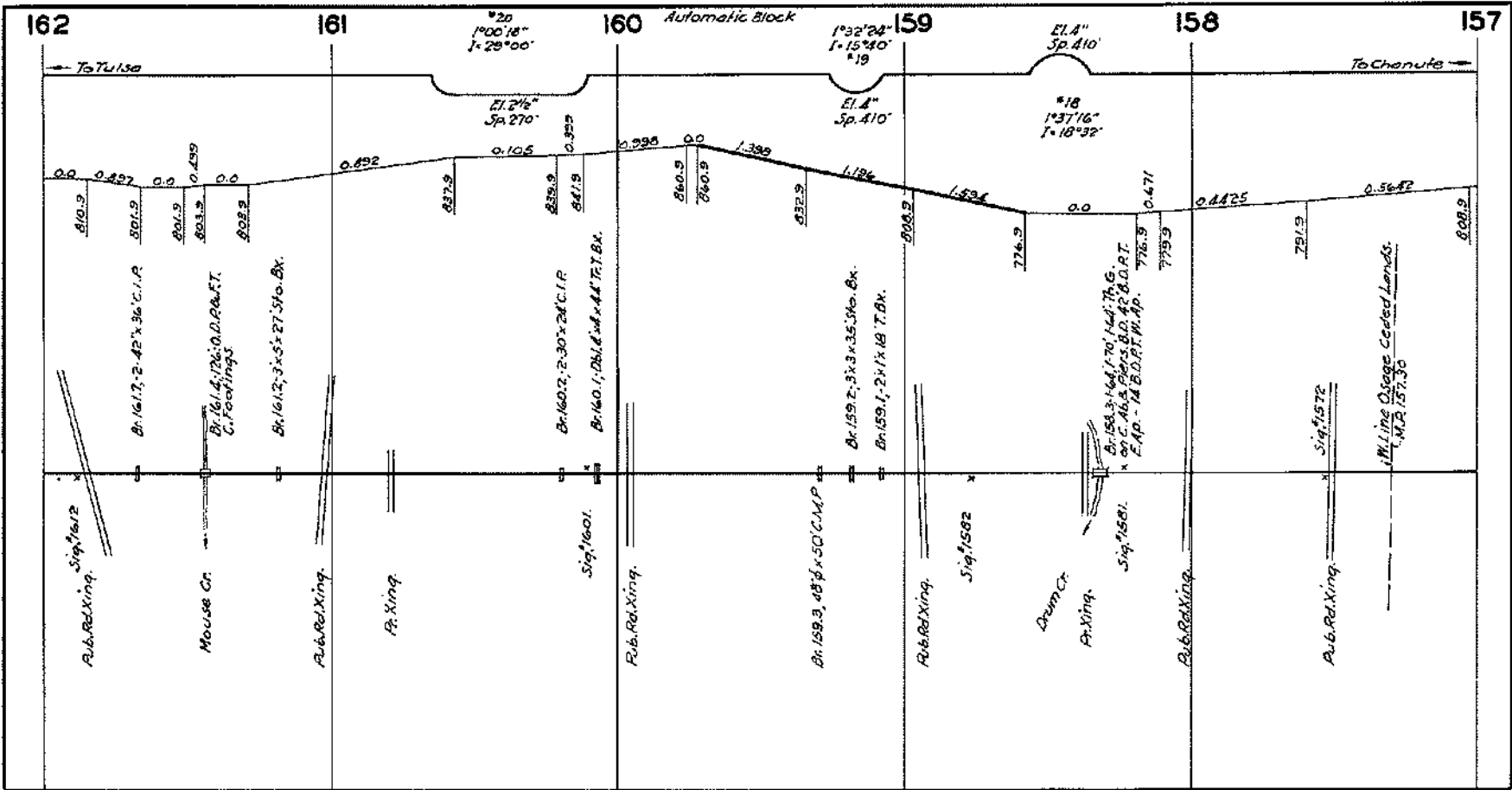
Pr. Xing.
Pub. Rd. Xing.
Pr. Xing.

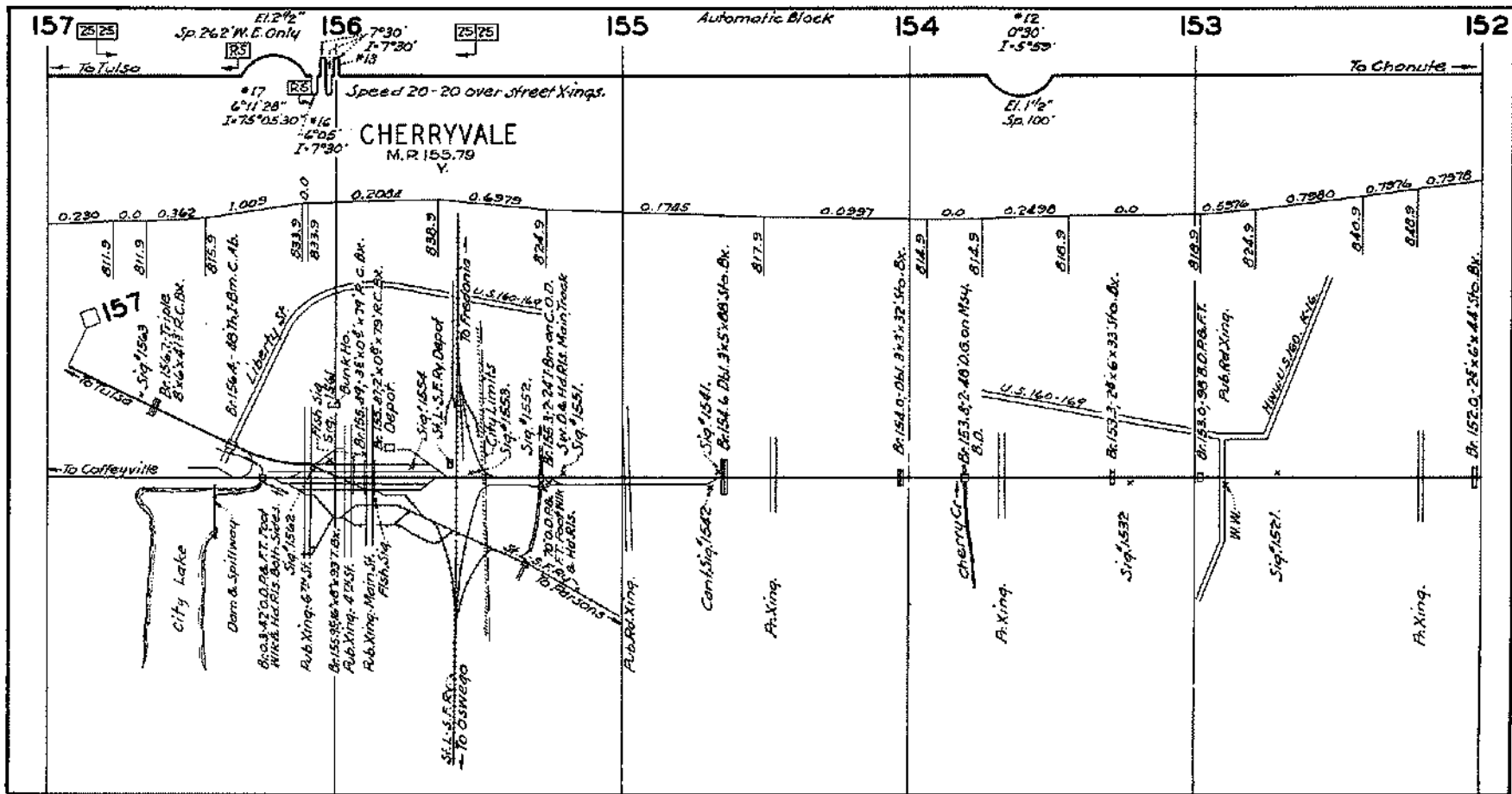
Section Shelter No.
Park St. Sig. #33.
Sta. Sign Parrish St.
Cant. Sig. #30.
Pub. Rd. Xing. State Hwy. 76.
Flth. Sig. #31.
To N. & W. Thompson.
Br. 1.2, 1-24 x 24 C.I.R.
Sig. #32.

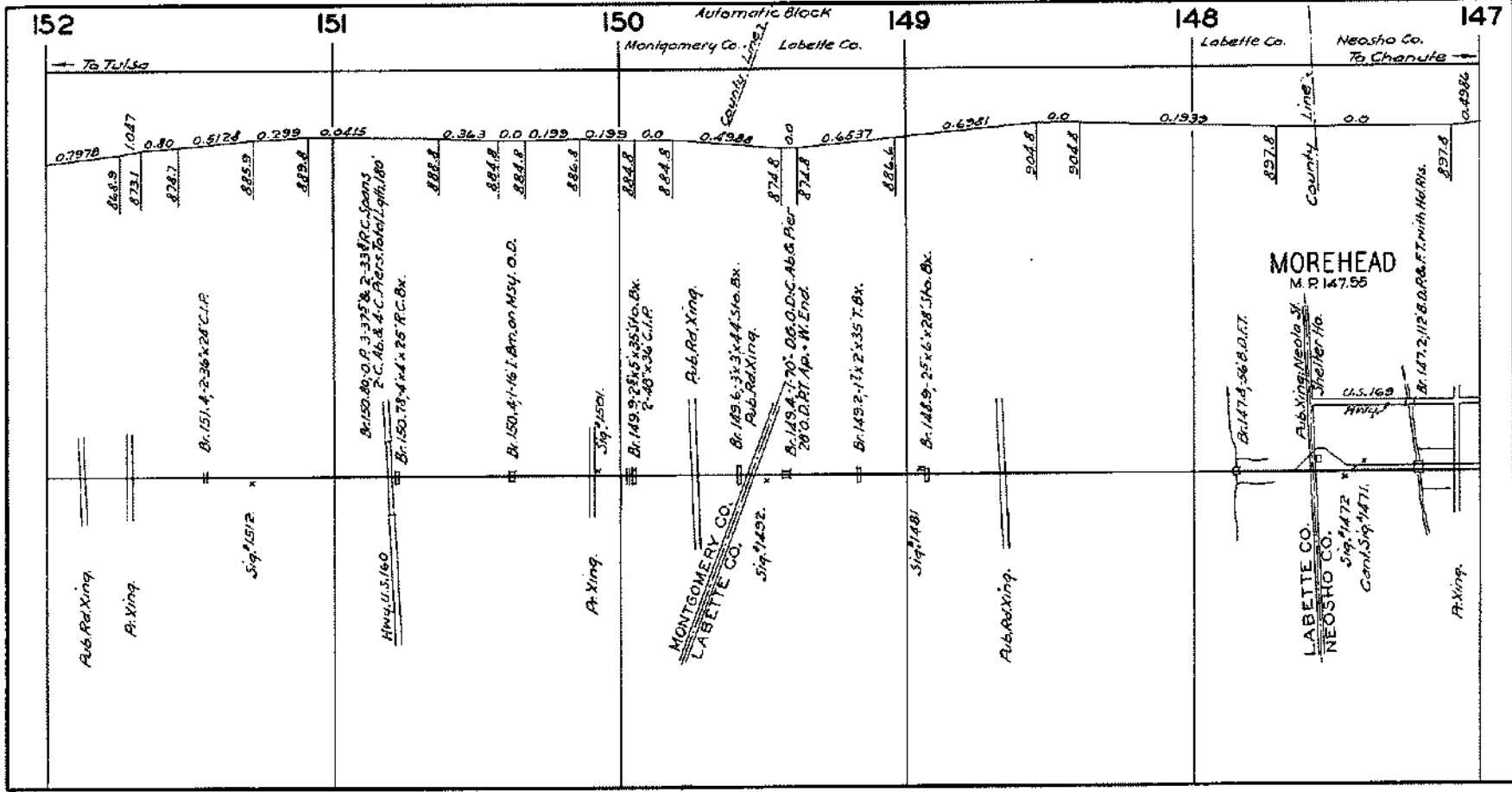
Drainage Ditch

Pr. Xing.
Pr. Xing.
Pub. Rd. Xing.
Pr. Xing.
Pr. Xing.

Miller
Pub. Rd. Xing. Engle Sig. #34.
To N. & W. Thompson.
C.A.B. B.D.
Pub. Rd. Xing.
1st Dist.
0.08 mi. Sec. 16
2nd Dist.







152

151

150

149

148

147

Automatic Block

To Tulsa

Montgomery Co. Labette Co.

Labette Co.

Neosho Co. To Charute

0.7978 0.80 0.5128 0.299 0.0415 0.363 0.0 0.199 0.199 0.0 0.4988 0.0 0.6537 0.6281 0.0 0.1932 0.0 0.4986

866.9 873.1 878.7 885.9 889.8 888.1 884.8 883.8 886.8 884.8 884.8 874.8 871.8 886.6 904.8 904.8 897.8 897.8

Br. 150-80-0-0 P. 3-375 x 2-338 R.C. Spans
2 C. Ab. & 4 C. Piers. Total Lght. 180'
Br. 150-78-4 x 4 x 28 R.C. Bx.
Br. 150-4-1-16 I. Brn. on Msy. O.D.
Br. 149-2-28-15 x 35 I. Ho. Bx.
2-48" x 36 C.I.P.
Br. 149-6, 3 x 3 x 4 I. Ho. Bx.
Br. 149-2, 1 x 2 x 35 T. Br.
20' O.D. R.T. Apr. - N. End.
Br. 148-9, 25 x 6 x 28 I. Ho. Bx.
Br. 147-4, 56 B.D.I.T.
Br. 147-2, 112 B.D.P. & F.T. with Ho. Ris.

Pub. Rd. Xing.

P. Xing.

Sig. 1512

Hwy. U.S. 160

P. Xing.

Sig. 1501

MONTGOMERY CO.
LABETTE CO.

Sig. 1492

Sig. 1481

Pub. Rd. Xing.

LABETTE CO.
NEOSHO CO.

Sig. 1472

Cont. Sig. 1471

P. Xing.

MOREHEAD

N. 147.55

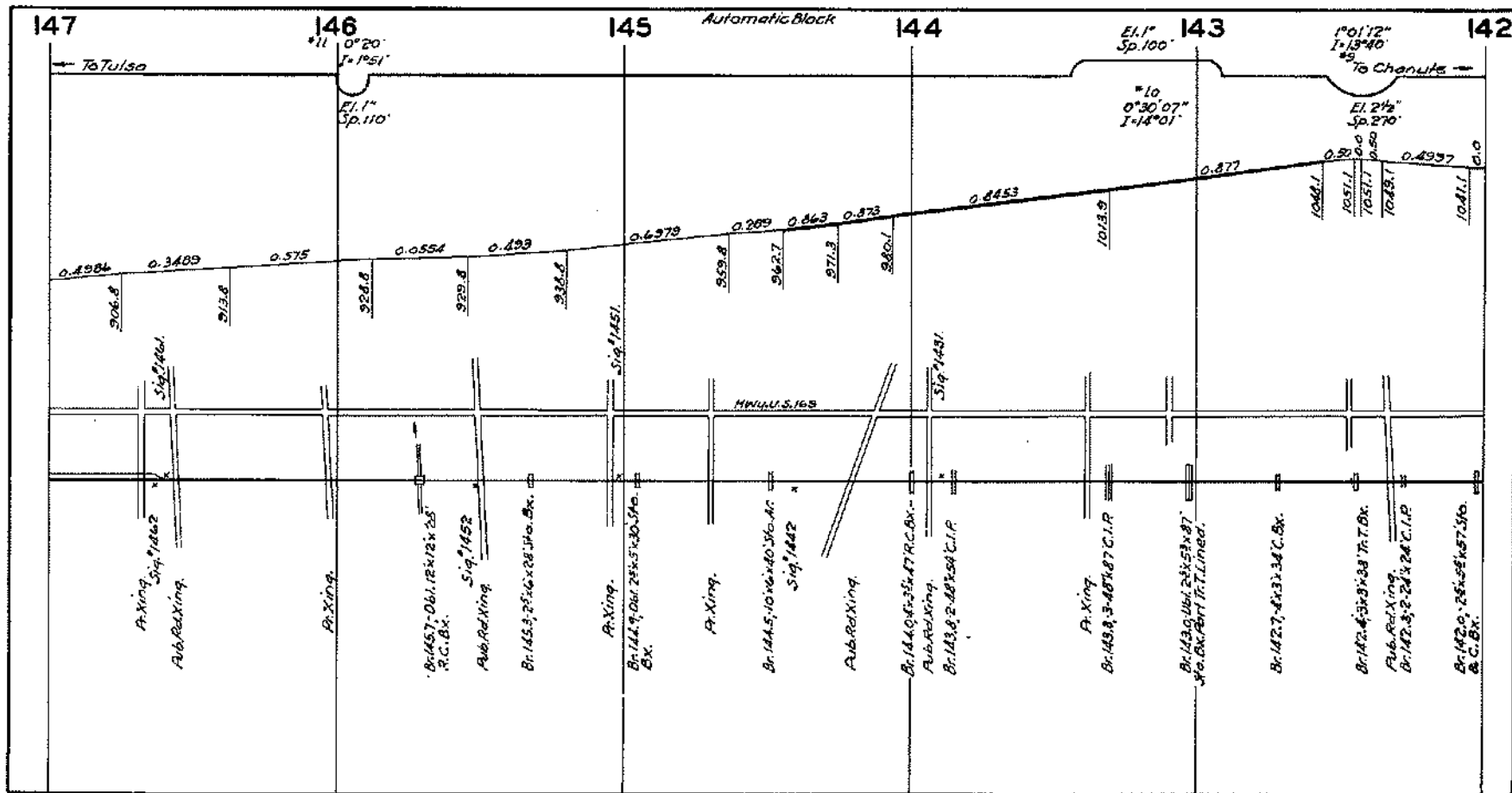
U.S. 160

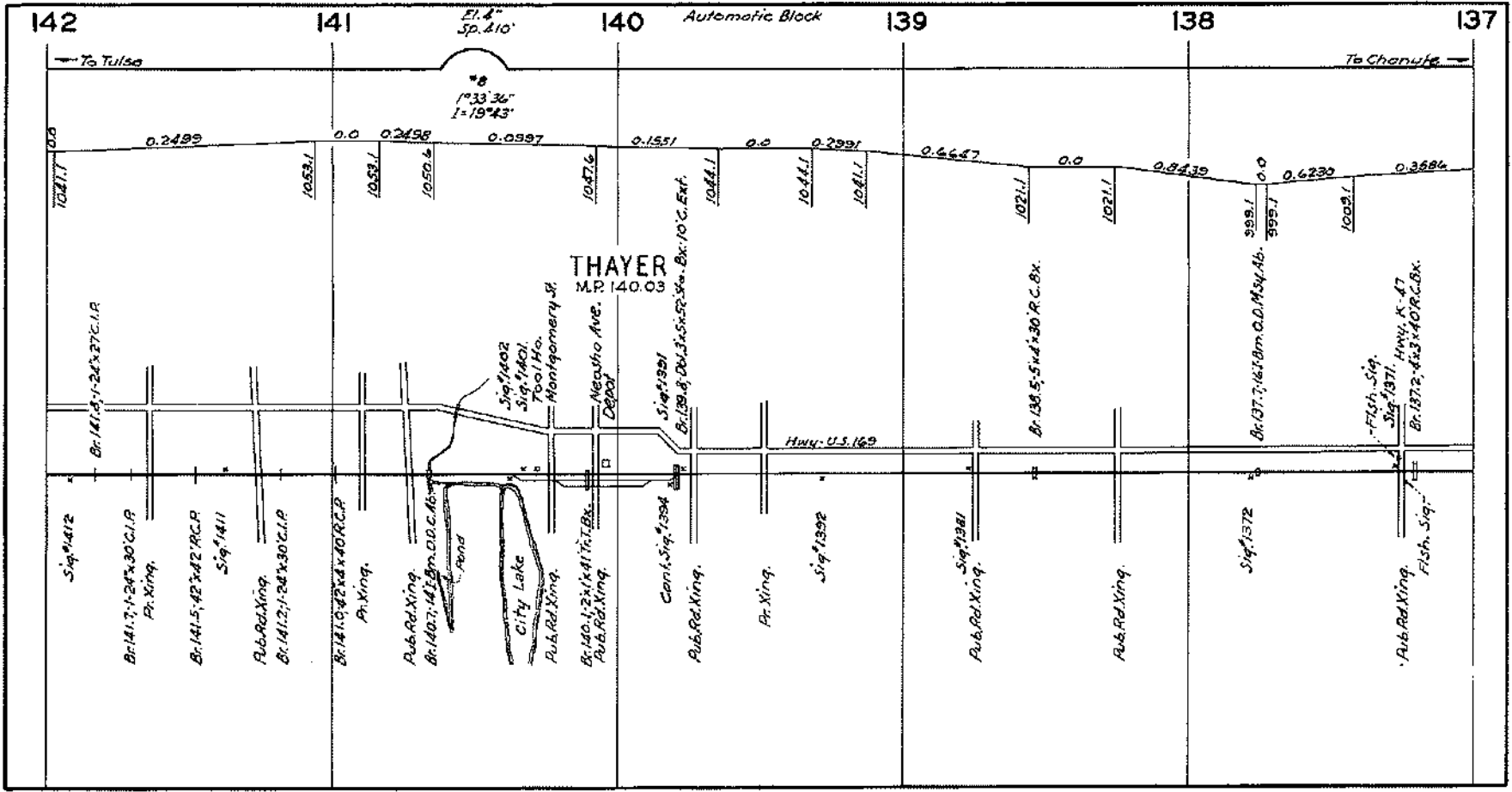
Hwy.

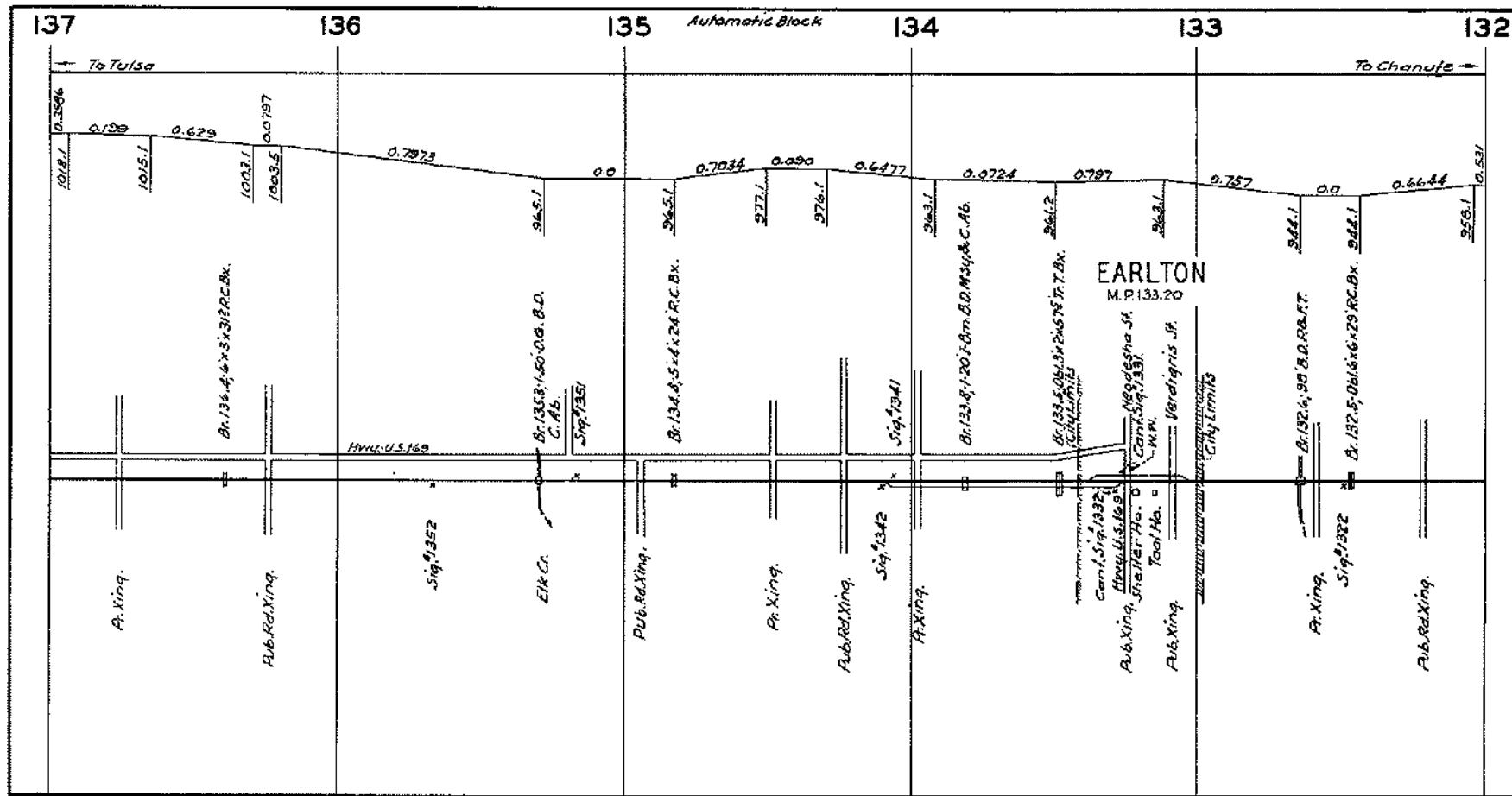
County Line

Pub. Xing. Neosho St. She/Hor Ho.

County Line



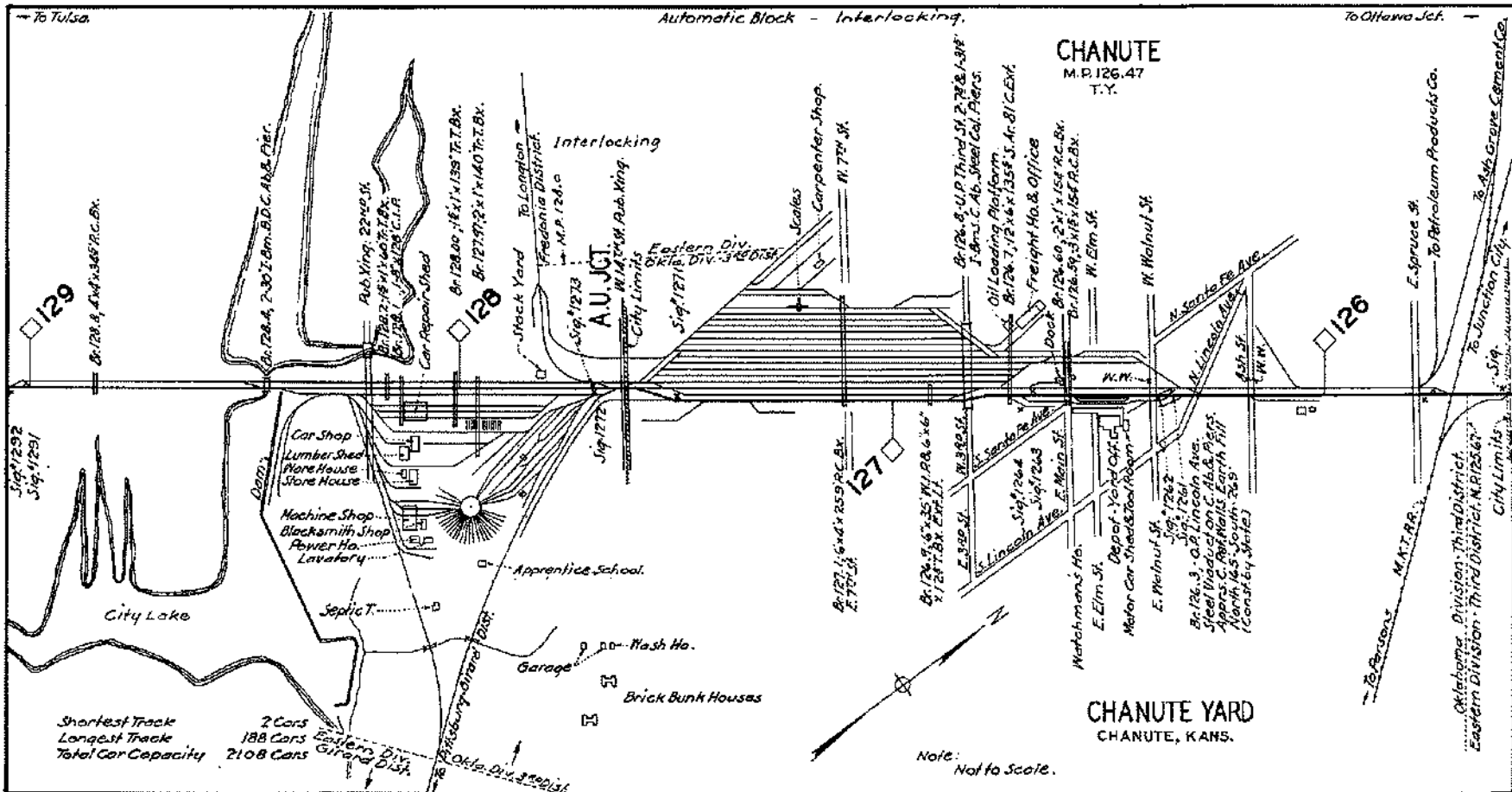




To Tulsa.

Automatic Block - Interlocking.

To Ottawa Jct.



Shortest Track
 Longest Track
 Total Car Capacity

2 Cars
 188 Cars
 2108 Cars

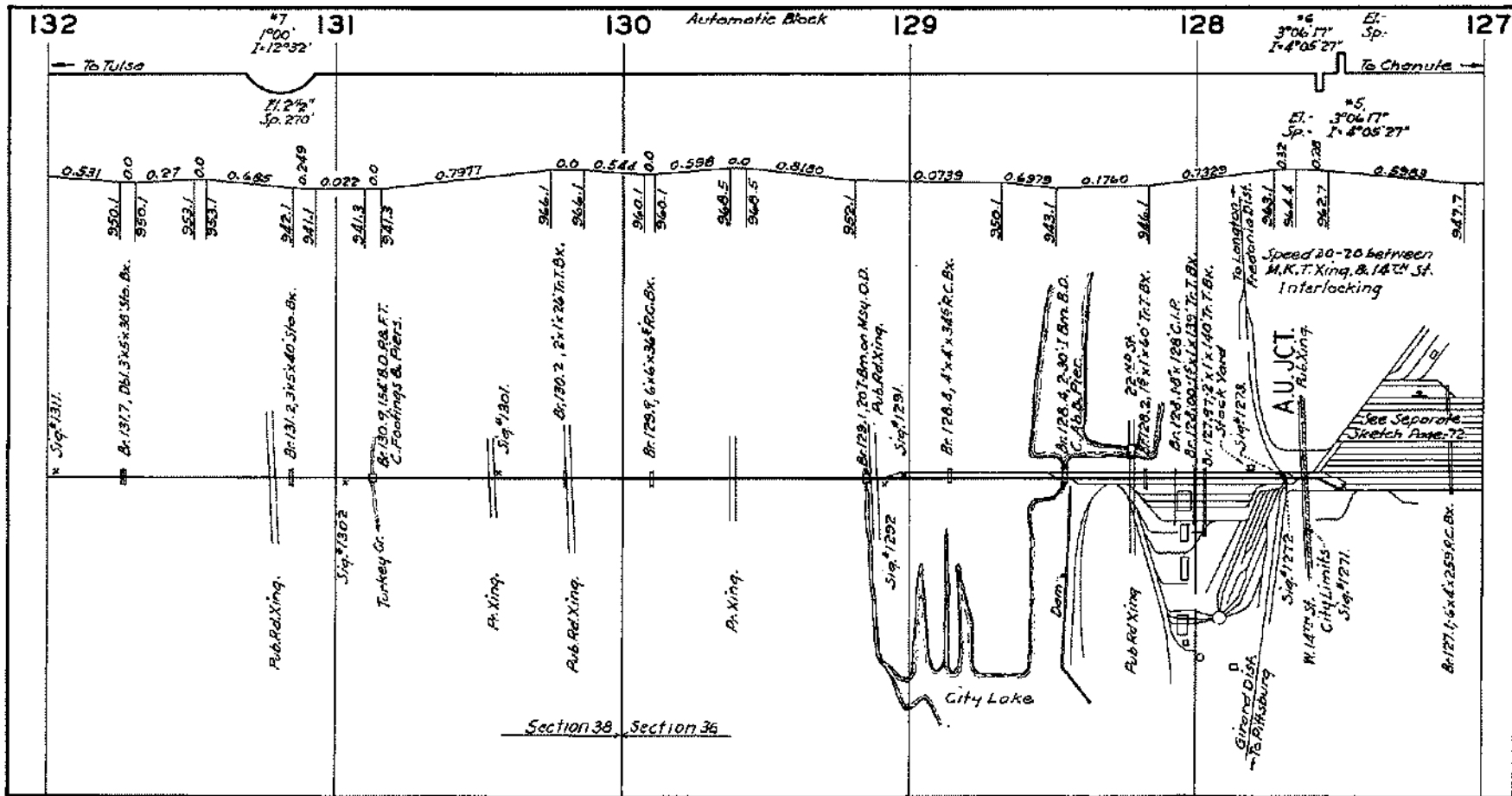
Eastern Div.
 Girard Dist.

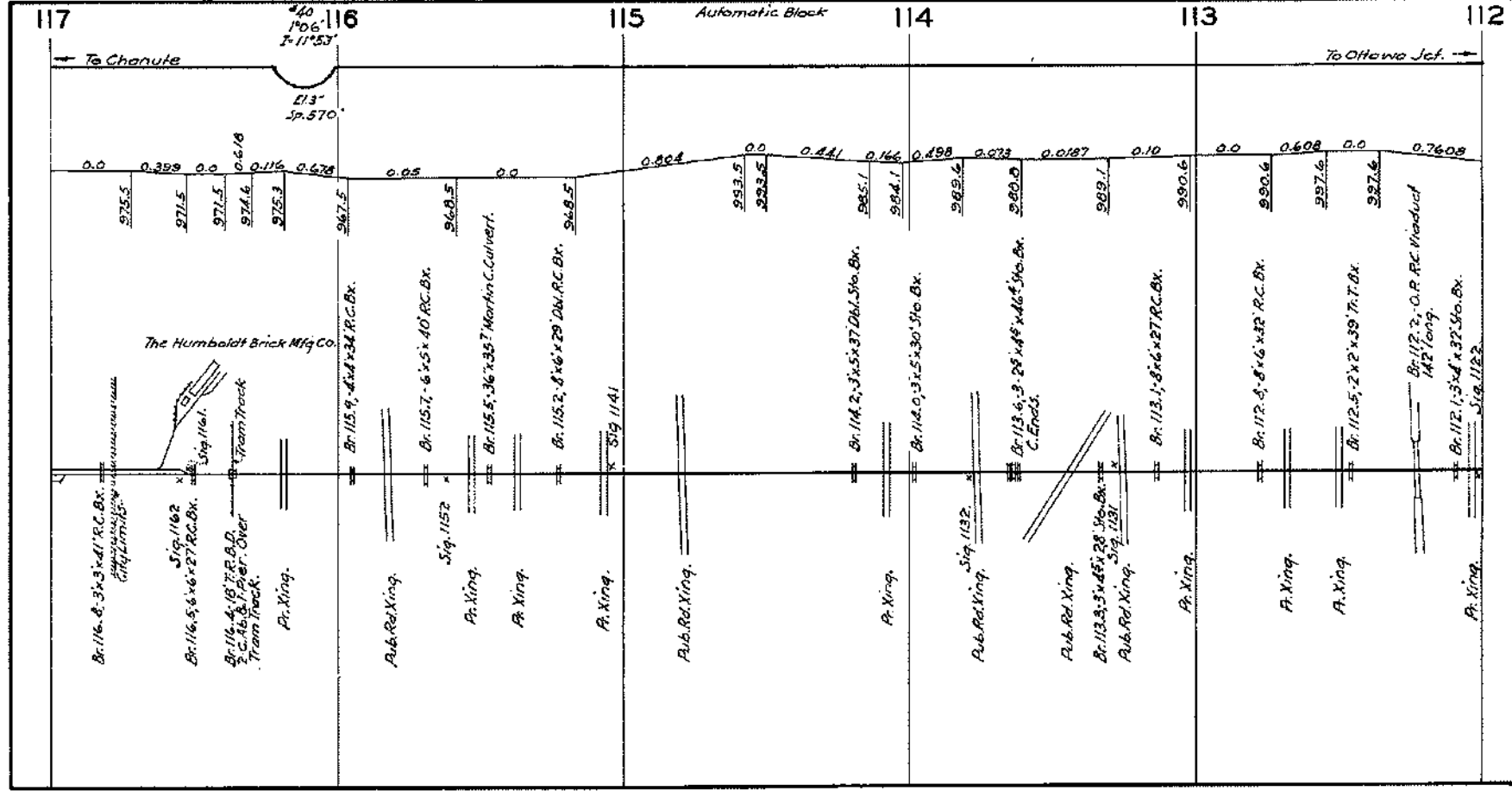
CHANUTE YARD
 CHANUTE, KANS.

Note:
 Not to Scale.

Oklahoma Division - Third District.
 Eastern Division - Third District - N. R. 125.67
 City Limits

To Ash Grove Cement Co.
 To Junction City.
 To Lawrence
 M.K.T. RR.
 To Parsons
 To Refractorium Products Co.
 To Ottawa Jct.





117

⁴⁰
106-116
I-11°53'

115

Automatic Block

114

113

112

→ To Charute

To Ottawa Jet. →

0.0 0.359 0.0 0.618 0.116 0.678

0.364 0.0 0.441 0.166 0.498 0.233 0.0187 0.10 0.0 0.608 0.0 0.7608

975.5

971.5

971.5

971.6

975.3

967.5

968.5

953.5

952.5

985.1

984.1

989.6

980.0

989.1

990.6

990.6

997.6

987.6

Br 116. 8. 3x3x41' R.C. Bx.
City Limits.

Sig. 1162
Br 116. 5. 6x6x27' R.C. Bx.

Br 116. 4. 16' x 16' D.
2 C. 16x16' Paper Over
Tram Track.

Pr. Xing.

Pub. Red Xing.

Sig. 1152
Br 115. 7. 6x5x40' R.C. Bx.

Pr. Xing.

Pr. Xing.

Pr. Xing.

Pub. Red Xing.

Pr. Xing.

Pub. Red Xing.

Sig. 1132
Pr. Red Xing.

Pub. Red Xing.

Br 113. 3. 3x45x28' Sto. Bx.
Pr. Red Xing.

Pr. Xing.

Pr. Xing.

Pr. Xing.

Pr. Xing.

Pr. Xing.

The Humboldt Brick Mfg Co.

Br 115. 9. 4x4x34' R.C. Bx.

Br 115. 7. 6x5x40' R.C. Bx.

Br 115. 5. 36x35' Martin C. Culvert.

Br 115. 2. 8x6x29' Dbl. R.C. Bx.

Sig 1141

Br 114. 2. 3x5x37' Dbl. Sto. Bx.

Br 114. 0. 3x5x30' Sto. Bx.

Br 113. 6. 3x29x48'x46' Sto. Bx.
C. Ends.

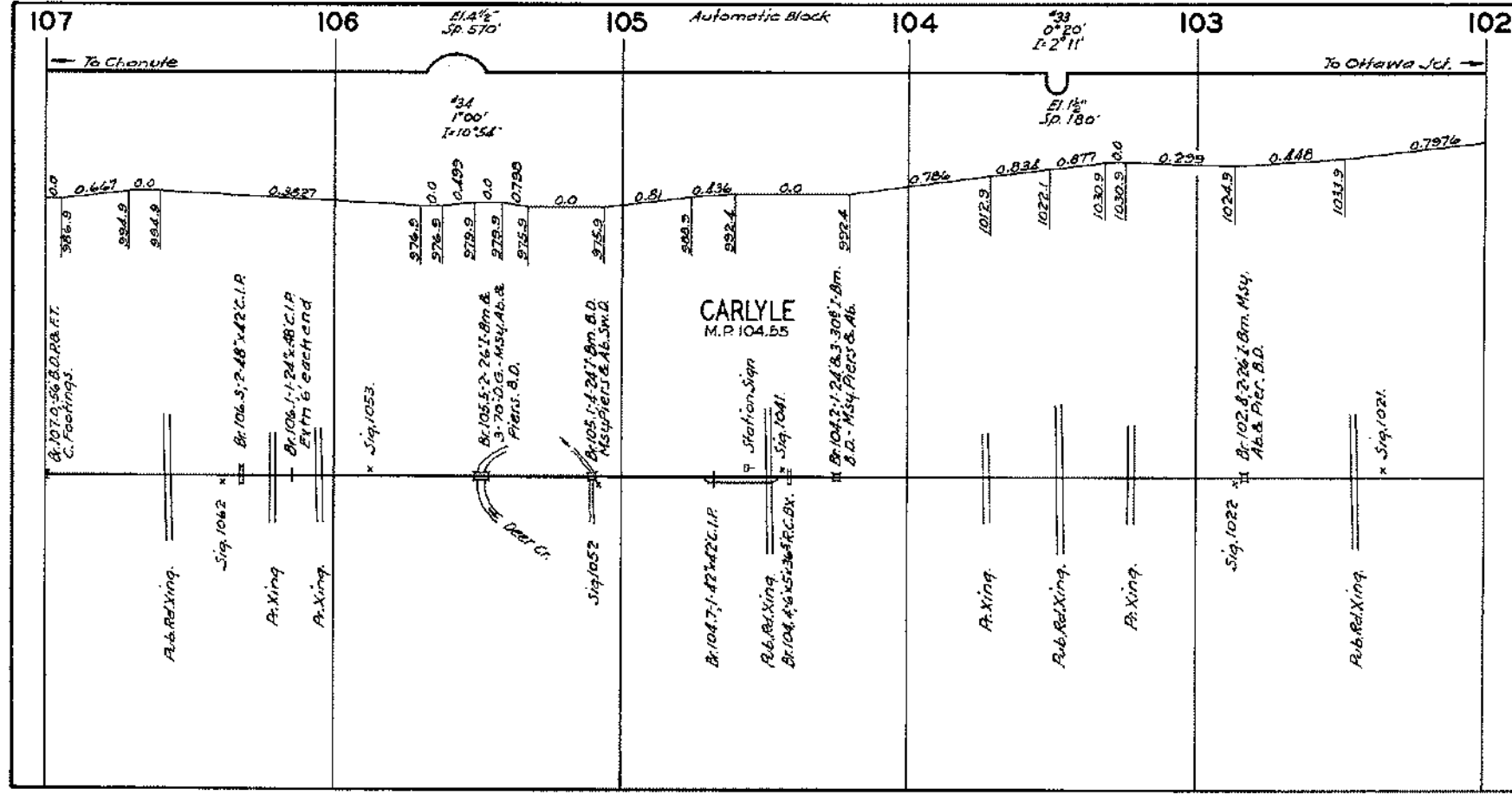
Br 113. 1. 8x6x27' R.C. Bx.

Br 112. 8. 8x6x32' R.C. Bx.

Br 112. 5. 2x2x39' T. Bx.

Br 112. 2. O.P. R.C. Viaduct
142' long

Br 112. 1. 3x4x37' Sto. Bx.
Sig. 1122



107

106

105

Automatic Block

104

E1 1/2
0° 20'
I=2° 11'

103

102

← To Charute

To Ottawa Jct. →

0.0 0.667 0.0 0.3227 0.0 0.499 0.0 0.790 0.0 0.81 0.836 0.0 0.786 0.834 0.877 0.0 0.299 0.448 0.0319 0.7976

Br. 107.0, 56' B.D. R.R. FT. C. Footings.

Br. 106.3, 2-48' x 22' C.I.P.

Br. 106.1, 1-24' x 48' C.I.P. Extra 6' each end

× Sig. 105.3.

Br. 105.5, 2-76' I.B.M. & 3-70' G. M.S.Y. Ab. & Piers. B.D.

Br. 105.1, 1-24' I.B.M. B.D. M.S.Y. Piers & Ab. Sm. G.

CARLYLE
M.P. 104.155

Station Sign

× Sig. 104.1.

Br. 104.2, 1-24' & 3-306' I.B.M. B.D. - M.S.Y. Piers & Ab.

Br. 102.8, 2-26' I.B.M. M.S.Y. Ab. & Pier. B.D.

× Sig. 102.1.

Ab. Rel. Xing.

A. Xing

A. Xing

Sig. 105.2

Br. 104.7, 1-42' x 22' C.I.P.

R. B. Rel. Xing

Br. 104.4, 2-15' x 36' R.C. BX.

A. Xing

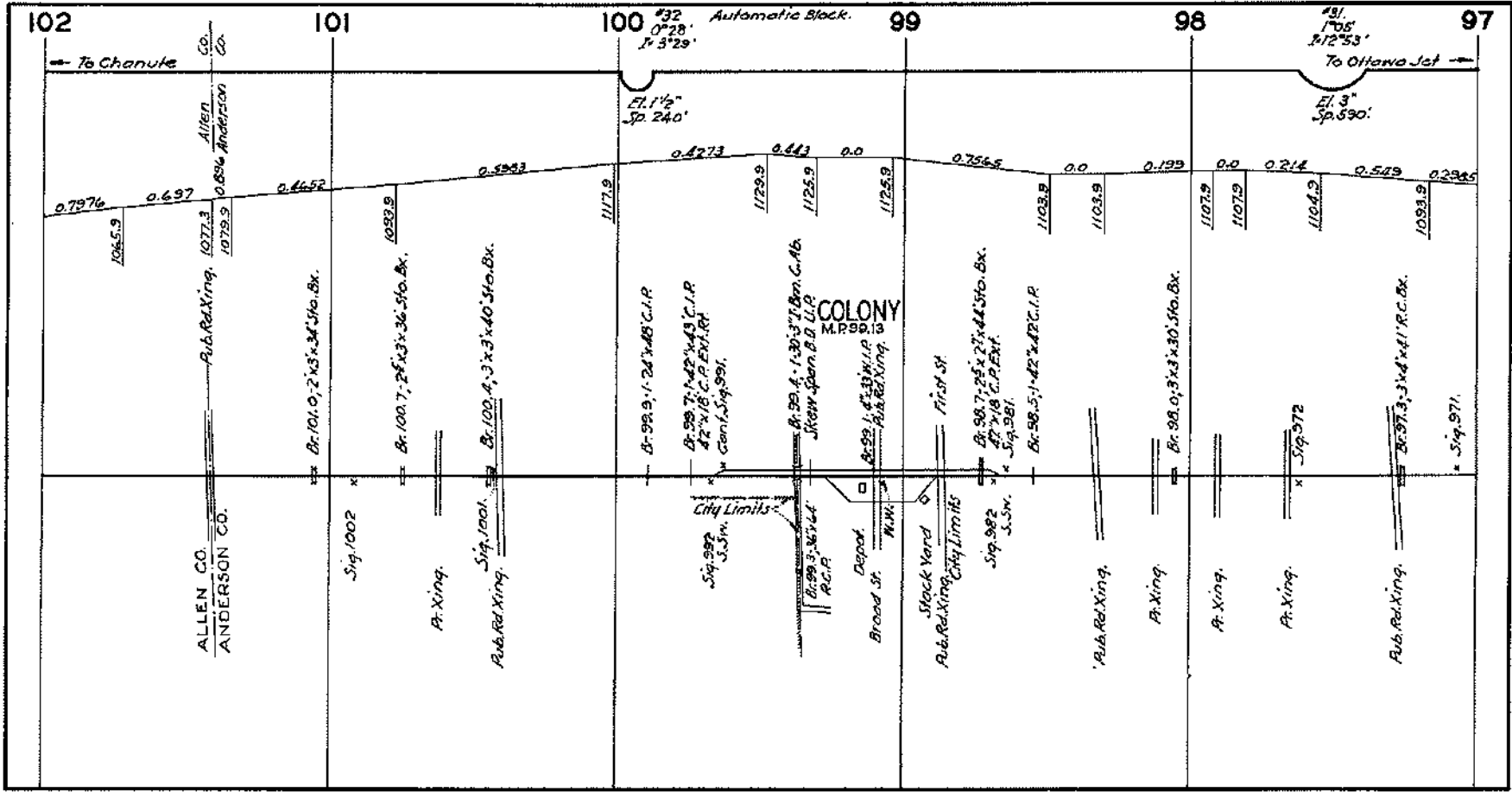
R. B. Rel. Xing.

A. Xing.

Sig. 102.2

R. B. Rel. Xing

Deer Cr.



102

101

100

99

98

97

Automatic Block.

32
 $0^{\circ}28'$
 $Sp. 3^{\circ}29'$

48
 $1^{\circ}05'$
 $Sp. 590'$

To Chanute

To Ottawa Jet

ALLEN CO.
ANDERSON CO.

Allen
Co.
Anderson
Co.

Sig. 1002

A. Xing.

Sig. 1001

Br. 101.0, 2 x 3 x 34 Sfo. Bx.

1030.9

Br. 100.7, 2 1/2 x 3 x 36 Sfo. Bx.

1171.9

Br. 99.9, 1-24 x 48 C.I.R.

Sig. 992
S.S.W.

City Limits

Br. 99.5, 36 x 44
R.C.R.

Depot

Broad St.

Br. 99.4, 1-30 x 37 1/2 Bm. C. Ab.

Stony Spn. B. U. C.I.R.

COLONY
M.P. 99.9

Stock Yard

Pub. Rd. Xing.
City Limits

First St.

Sig. 992
S.S.W.

Br. 98.7, 25 x 27 x 44 Sfo. Bx.

47 x 18 C.P. EXT.
x Sig. 981.

1125.9

Br. 99.9, 47 x 18
Pub. Rd. Xing.

Br. 98.5, 1-42 x 42 C.I.R.

1103.9

Pub. Rd. Xing.

A. Xing.

Br. 98.0, 3 x 30 x 30 Sfo. Bx.

1103.9

A. Xing.

A. Xing.

Sig. 972

1107.9

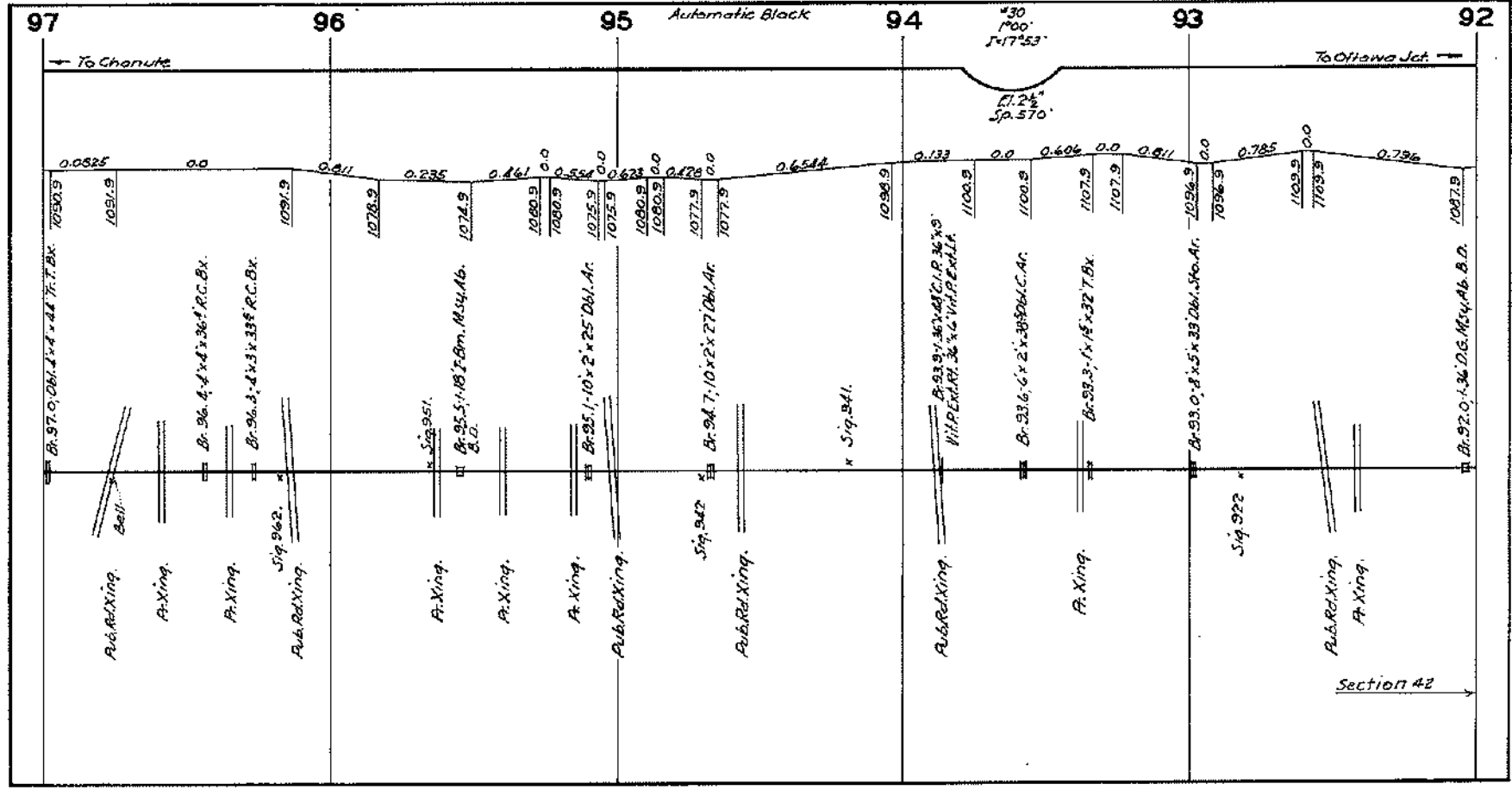
1107.9

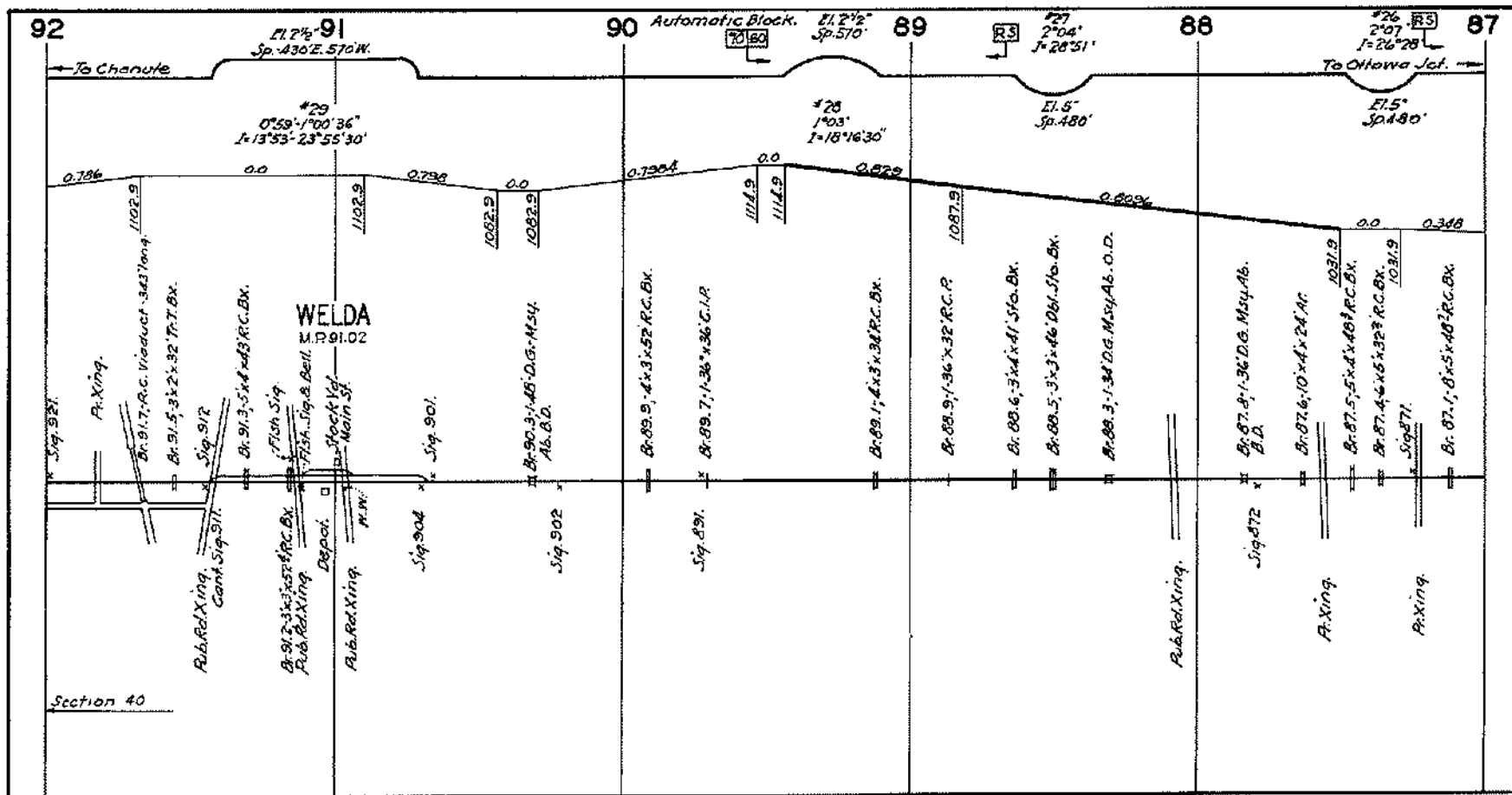
Br. 97.3, 3 x 4 x 41 R.C. Bx.

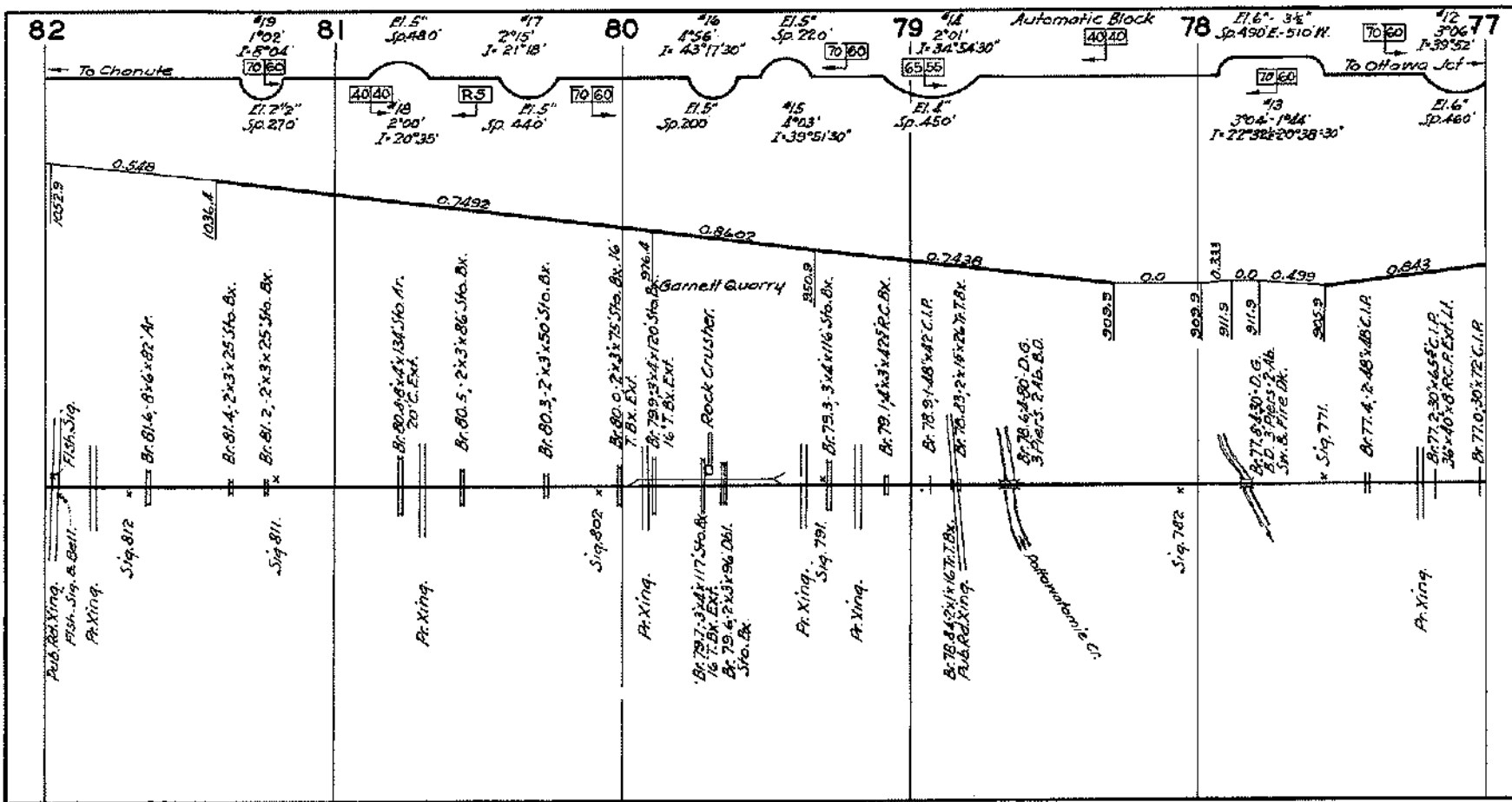
1104.9

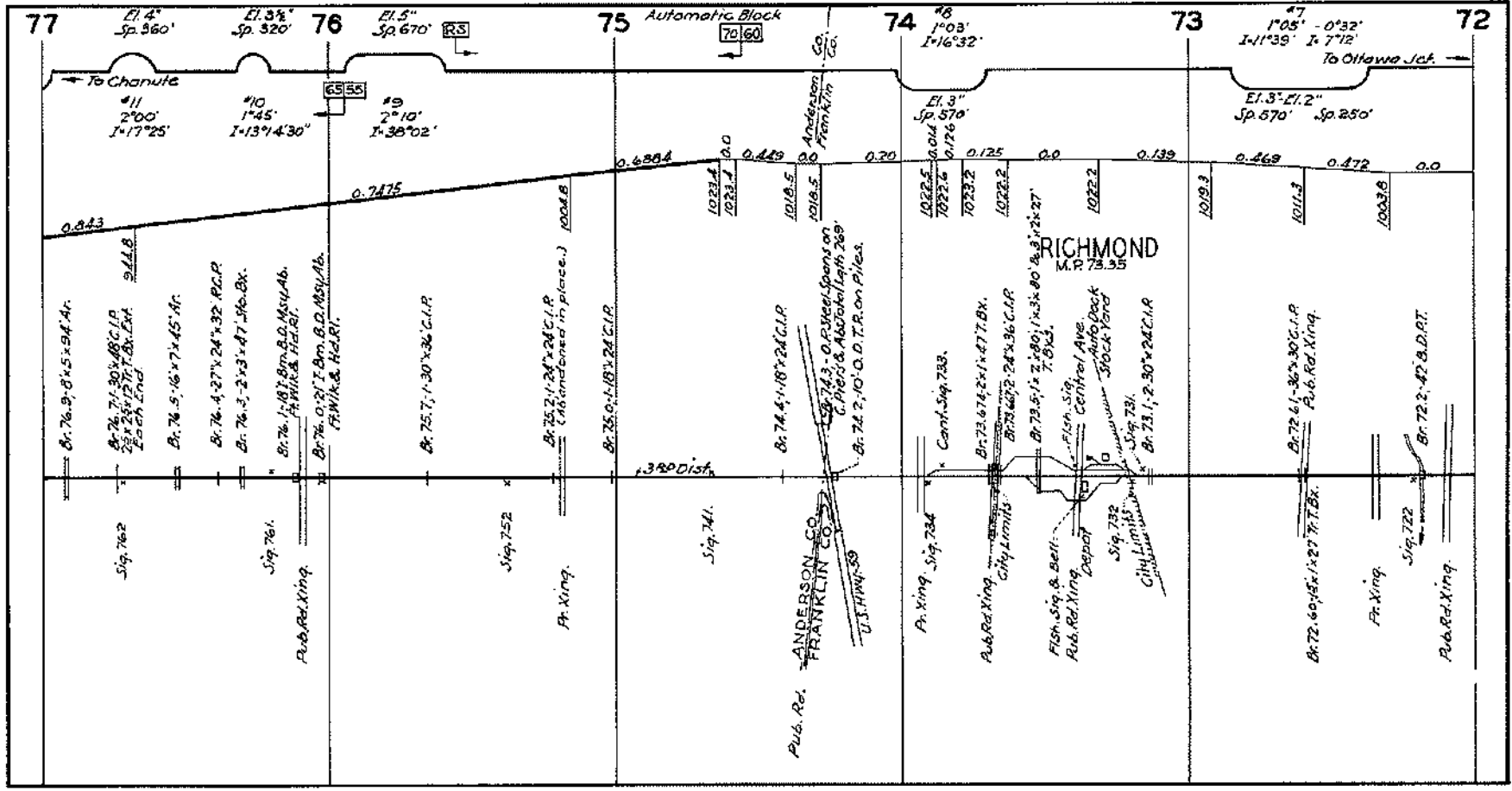
Sig. 971

1093.9









77

El. 4' Sp. 360'

76

El. 3 1/2' Sp. 320'

75

El. 5' Sp. 670'

74

Automatic Block
El. 3' Sp. 570'

73

El. 3' Sp. 570'

El. 3' Sp. 570'

72

To Chanute

#11
2'00'
I-17'25"

#10
1'45'
I-13'14'30"

#9
2'10'
I-36'02"

6555

7060

Anderson Franklin Co.

0.043

0.7475

0.6884

0.0

0.449

0.20

El. 3' Sp. 570'

0.125

0.0

0.139

El. 3' Sp. 250'

0.469

0.472

0.0

Br. 76.9.8x5x94 Ar.

Br. 76.7.1.30x48 C.I.P.
2 1/2 x 2 1/2 x 17 1/2 T.Bx. Ext. Each End.

Br. 76.5.16x7x45 Ar.

Br. 76.4.27x24x32 R.C.P.

Br. 76.3.2x3x47 5/8 Bx.

Br. 76.1.18x8 B.D. 4x4 Ab.
FRANK & H.L.P.

Br. 76.0.21x8 B.D. 4x4 Ab.
FRANK & H.L.P.

Br. 75.7.1.30x36 C.I.P.

Br. 75.2.1.24x24 C.I.P.
(Abandoned in place.)

Br. 75.0.1.18x24 C.I.P.

Br. 74.4.1.18x24 C.I.P.

Br. 74.3.0. Piles Spans on C. Piles & Abutments 269

Br. 74.2.10.0. D.T.R. on Piles.

Conf. Sig. 733.

Br. 73.6.7.6x1x47 Bx.

Br. 73.6.7.2.24x36 C.I.P.

Br. 73.5.1x2.180; 1x3x80 1/2x8x27

Fish. Sig. Center Ave.

Pub. Rel. Xing. Auto Deck

Sig. 732

City Limits

Br. 73.1.2.30x24 C.I.P.

Br. 72.6.1.36x30 C.I.P.

Pub. Rel. Xing.

Br. 72.2.47 B.D.P.T.

Sig. 762

Sig. 761

Pub. Rel. Xing.

Sig. 752

Fr. Xing.

Sig. 741

Pub. Rel.

ANDERSON CO.
FRANKLIN CO.
U.S. HIGHWAY

Fr. Xing. Sig. 734

Pub. Rel. Xing.

Fr. Xing.

Pub. Rel. Xing.

Sig. 732

City Limits

Fr. Xing.

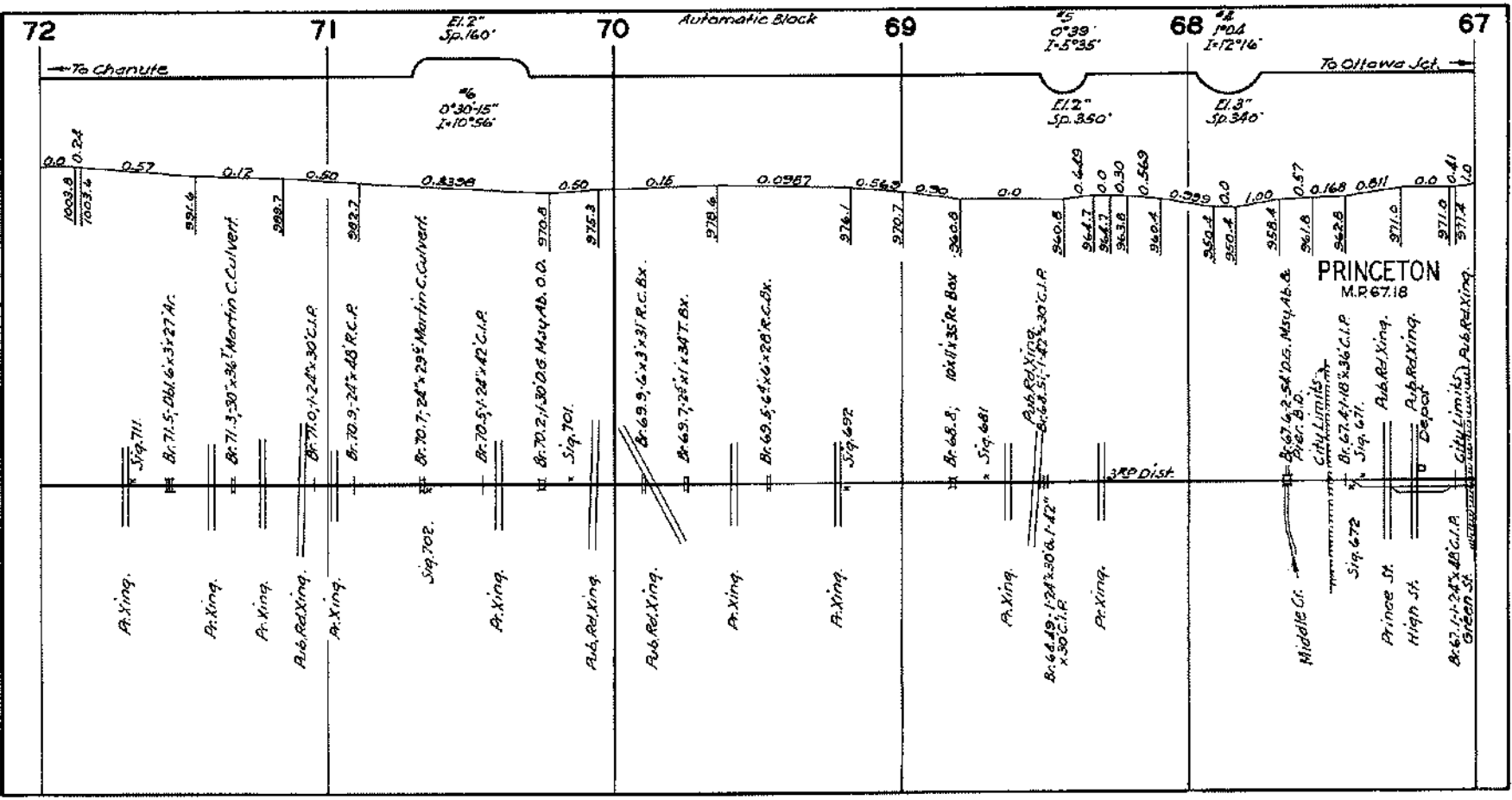
Sig. 722

Pub. Rel. Xing.

RICHMOND
K. 10 73.35

1320 Dist.

To Ottawa Jct



72

71

70

69

68

67

Automatic Block

To Charulte

To Ottawa Jct.

El. 2" Sp. 160'

El. 2" Sp. 350'

El. 3" Sp. 340'

1/2 0°30'15" 1-10°56'

1/2 0°39' 1-5°35'

1/2 1°12'16"

1003.8
7003.4

0.57

0.17

0.50

0.4398

0.50

0.16

0.0987

0.563

0.30

0.0

0.449

0.0

0.30

0.569

0.999

0.0

1.00

0.57

0.168

0.61

0.0

0.41

1.0

A. Xing.
Sig. 711.
Br. 71.5, 261.6 x 3 x 27 Ar.

A. Xing.
Br. 71.3, 30 x 36 Martin C. Culvert

A. Xing.
Pub. Red Xing.
Br. 71.0, 1-24 x 30 C.I.P.

A. Xing.
Br. 70.9, 24 x 48 R.C.P.

Sig. 702.
Br. 70.7, 24 x 29 1/2 Martin C. Culvert

A. Xing.
Br. 70.5, 1-28 x 42 C.I.P.

Pub. Red Xing.
Br. 70.2, 1-30 x 06 Msy 4 Ad. O.O. 970.8
Sig. 701.

Pub. Red Xing.
Br. 69.9, 6 x 8 x 31 R.C. Bx.

A. Xing.
Br. 69.7, 25 x 1 x 34 T. Bx.

A. Xing.
Br. 69.5, 6 x 6 x 0 x 28 R.C. Bx.

A. Xing.
Sig. 692

Br. 68.8, 10 x 11 x 35 Re Box

A. Xing.
Sig. 681

Pub. Red Xing.
Br. 68.5, 1-42 x 30 C.I.P.

3rd Dist

A. Xing.

Middle Cr.
Br. 67.6, 2, 54 x 05, Msy. 16. 6
Per. B.O.

City Limits
Br. 67.4, 1-18 x 36 C.I.P.

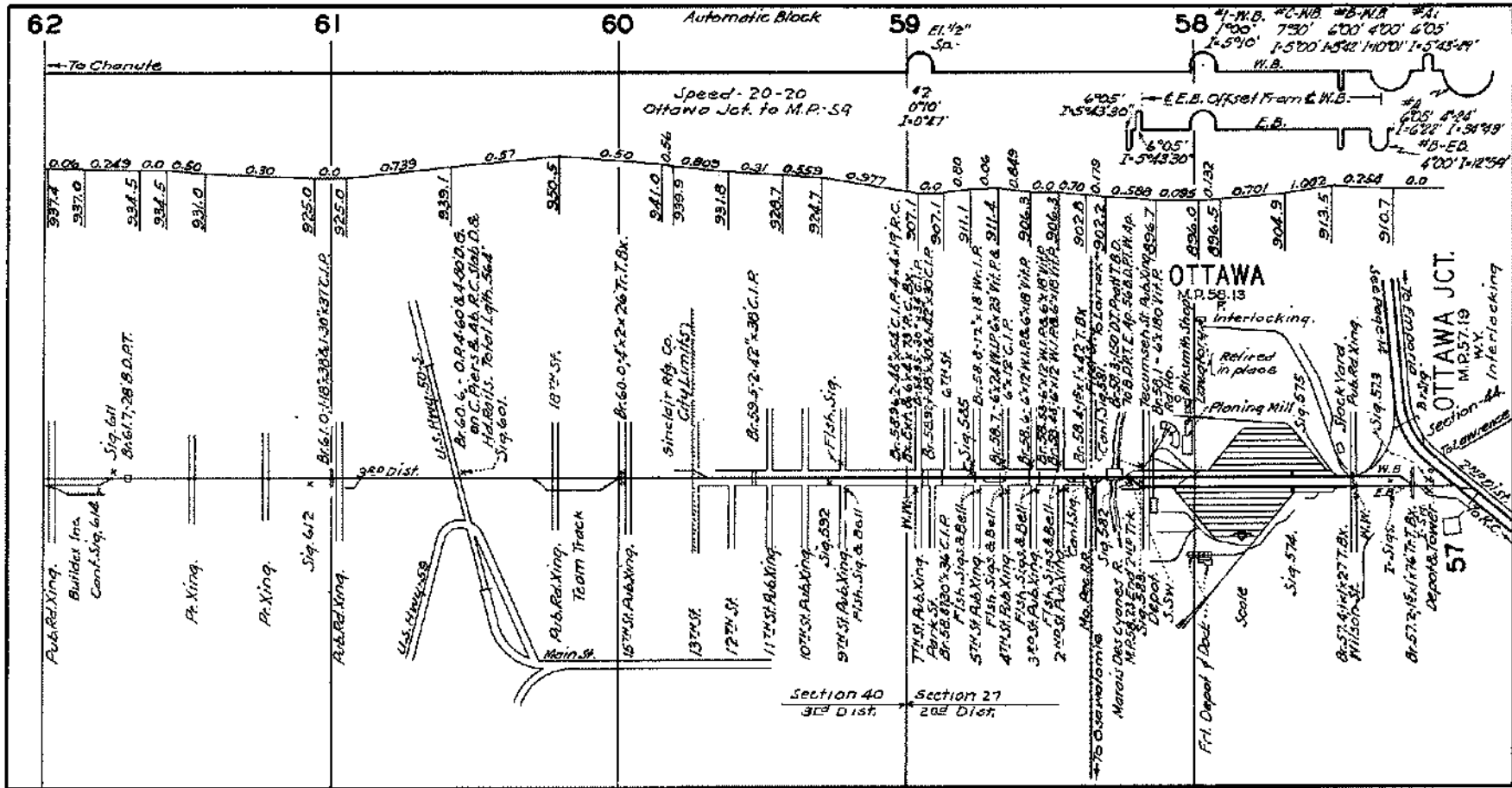
Sig. 672

Prince St.
Pub. Red Xing.

High St.
Pub. Red Xing.
Depot

Br. 67.1, 1-24 x 48 C.I.P.
Green St.
City Limits
Pub. Red Xing.

PRINCETON
M.D. 67.18



62

61

60

Automatic Block

59

58

Speed - 20-20
Ottawa Jct. to M.P. 59

E.E.B. Offset Form & W.B.
F.B.

OTTAWA

OTTAWA JCT.
M.P. 57.19
W.Y.
Interlocking

Section 40
3rd Dist.

Section 27
2nd Dist.

57

Automatic Block 115

To Newton To Topeka

0.0 0.01473

1120.1

Interlocking

MERRICK M.R. 115.30

M.R. 115.64.1

Br. 115.5-70 B.D.P.T. - S.S.C.
Br. 115.4 Kan. Turripike
Overpass 209 - 88 Spans
Signal Cir. RC. 116's, and
Bents. 2-28 Roadways
Br. 119

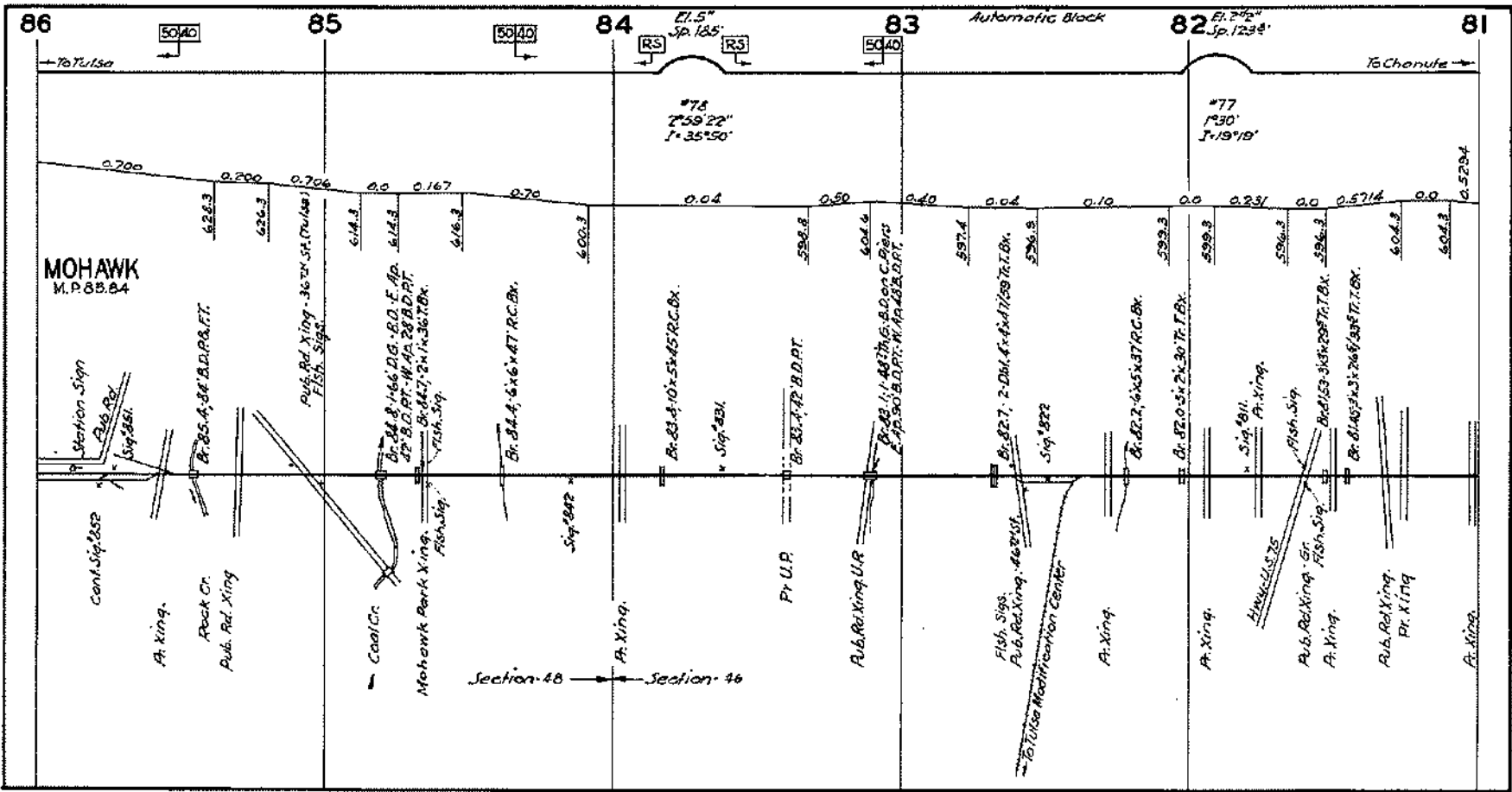
H.B.M.
L.B.M.
3RD Main
1ST Dist.

Land Limit Sign

Signal Tower
M.B. 3.30 P.O. Bell
Pub. Rel. Xing

Middle Division 1ST District Eastern Division 1ST District

Section-1. Section-11



MOHAWK
W.P. 83.84

86

85

84

83

82

81

Automatic Block

E1.5°
Sp. 185'
#76
7°59'22"
I = 35°50'

E1.7°
Sp. 123'
#77
1°30'
I = 19°48'

→ To Tulsa

To Cherokee →

0.700 0.200 0.706 0.0 0.167 0.70 0.04 0.50 0.40 0.04 0.10 0.0 0.231 0.0 0.5714 0.0 0.5294

Station Sign
Pub. Rd.
Sig 1551

Br. 85.4-36' B.D.P. & FT.

Pub. Rd. Xing - 36' x 34' (7/16/15)
Fish Sign

Br. 84.8-166' D.G. - B.O. E. Ad.
27' B.D.P.T. - W. Ad. 28' B.D.P.T.
Br. 84.7-24' x 36' T.O.B.
Fish Sign

Br. 84.4-6' x 6' x 47' R.C.B.

Br. 83.4-10' x 5' x 45' R.C.B.
v Sig 481

Br. 83.4-42' B.D.P.T.

604.6
Br. 83.1-143' H.G. - A.D. on C. Diers
E. Ad. 50' B.D.P.T. - W. Ad. 28' B.D.P.T.

597.4
Br. 82.7-2' D.H.L. 4' x 4' x 1750' T.O.B.
596.9
Sig 1522

Br. 82.2-6' x 5' x 37' R.C.B.

Br. 82.0-5' x 2' x 30' T.O.B.

Sig 481
A. Xing

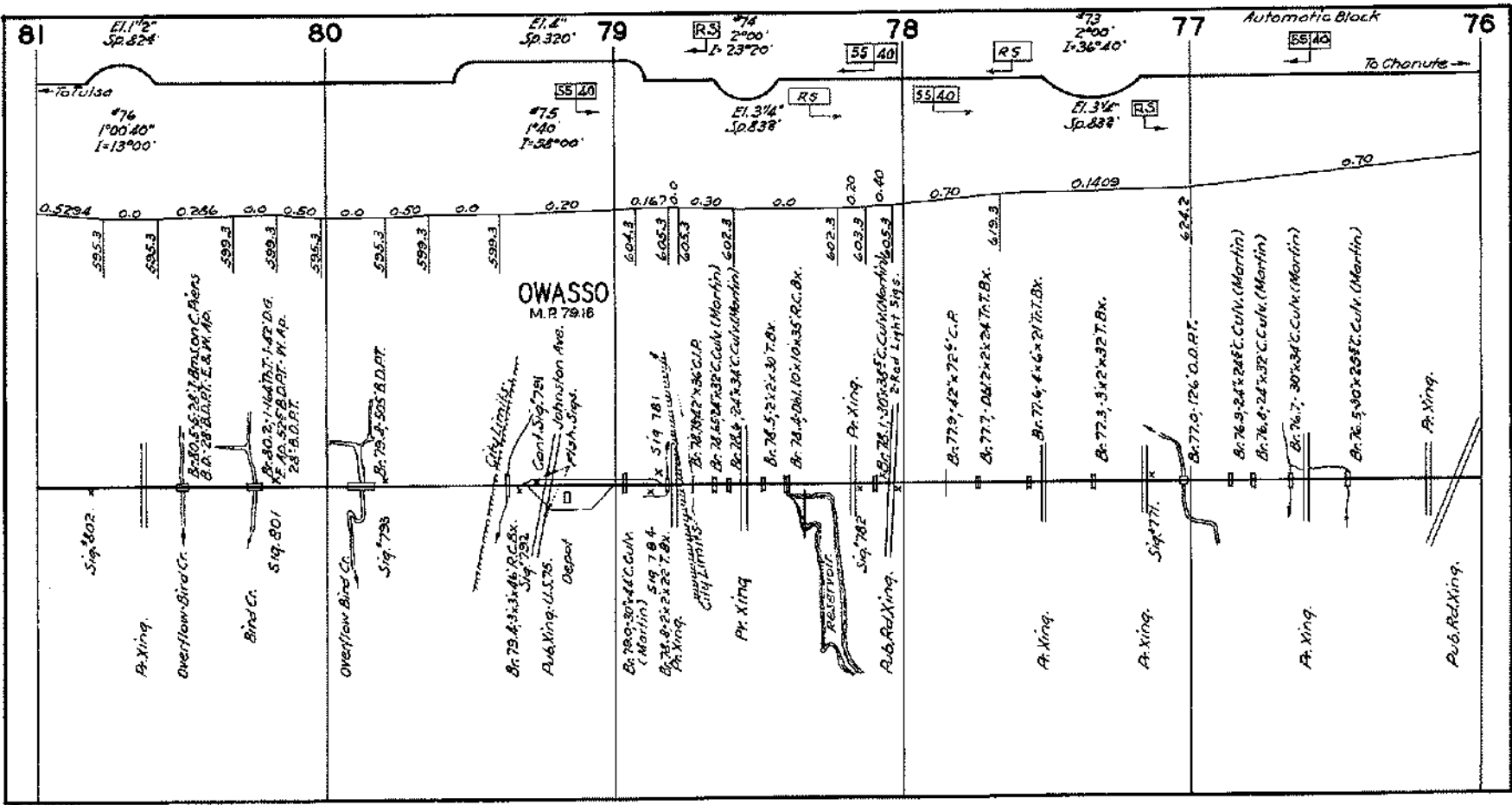
Fish Sign

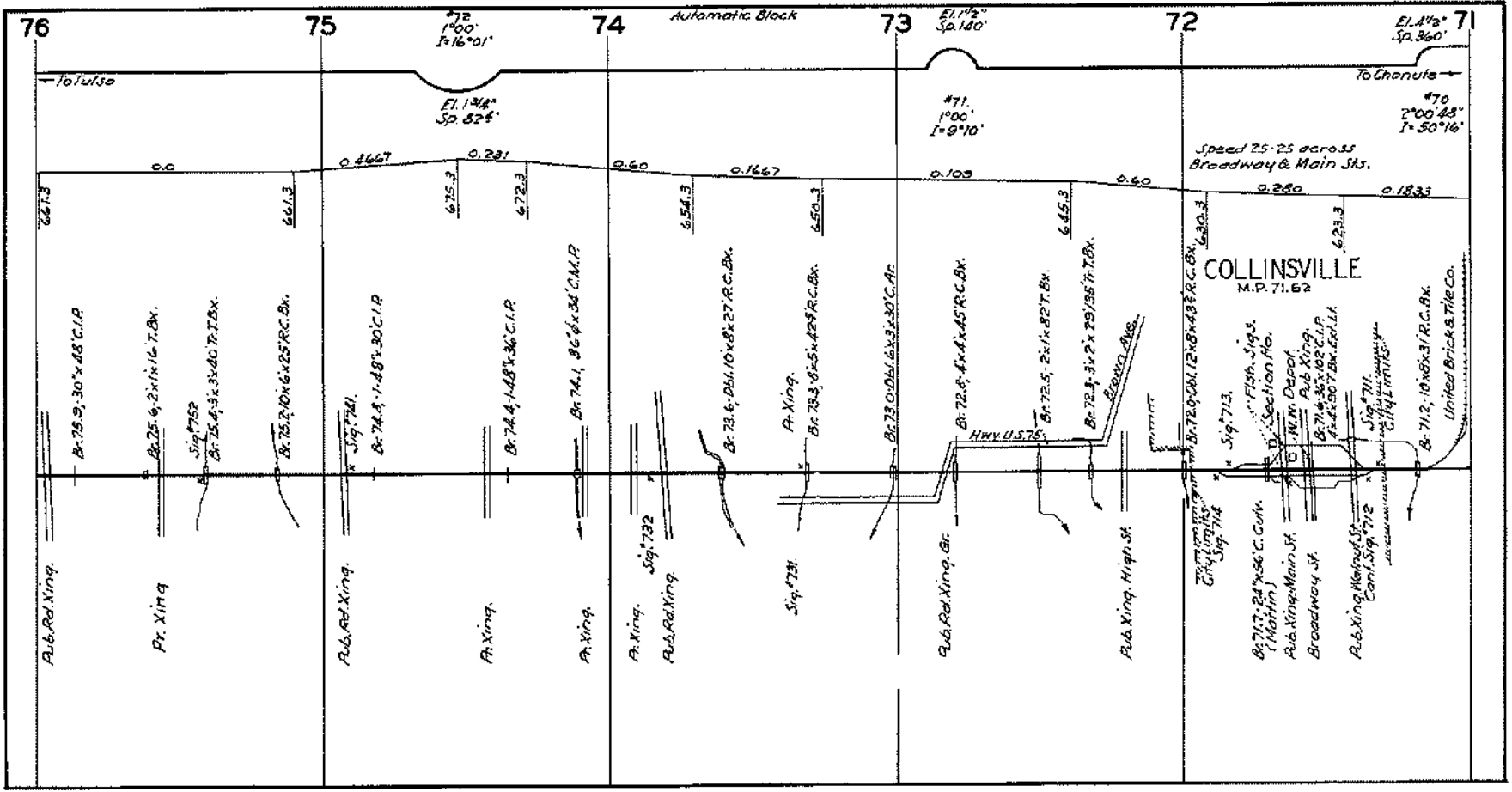
596.3
Br. 81.5-3' x 3' x 28' T.O.B.
594.3
Br. 81.4-3' x 2' x 30' T.O.B.

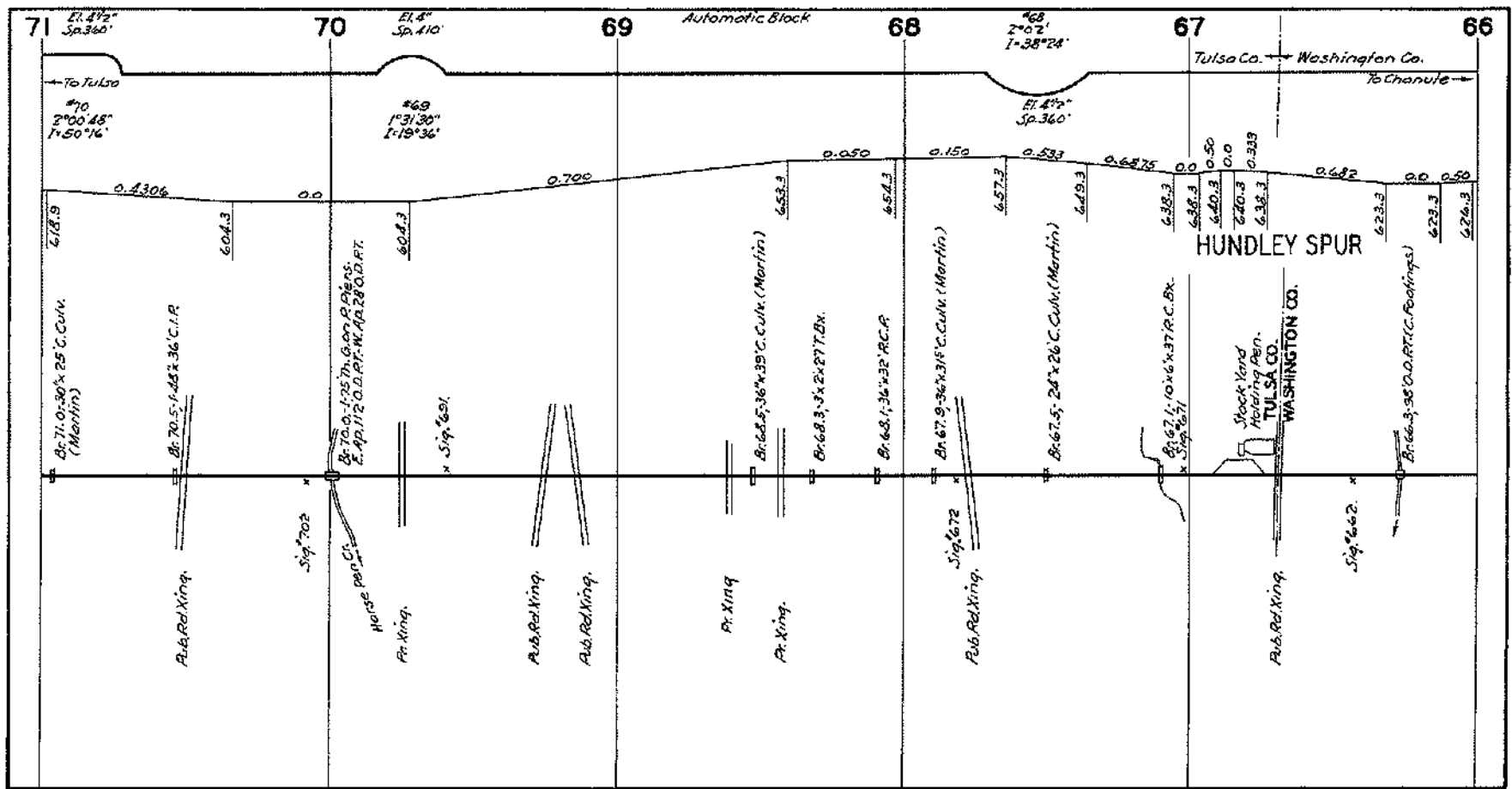
Pub. Rd. Xing
Pr. Xing

A. Xing

Section 48 Section 46







71 EL. 472' Sp. 360' 70 EL. 4' Sp. 110' 69 Automatic Block 68 EL. 468' 2'02' 1-38'28" 67 66

To Tulsa To Chanute

71 2'00'48" 1-50'76" 70 #69 1'31'30" 1-19'36" 68 EL. 472' Sp. 360' 67 0.50 0.0 0.333 0.682 66 0.50

0.4306 0.0 0.729 0.050 0.150 0.533 0.6875 0.0 0.682 0.0 0.50

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

Br. 71.0-30' x 25' C. CULV. (Marlin) Br. 70.5: 145' x 36' C.I.P. Br. 70.0-125' x 36' C.I.P. S. & AP. 120.0 RT. W. AR. 20.0 RT. Br. 68.5: 36' x 39' C. CULV. (Marlin) Br. 68.3: 3' x 2' x 27' T. BX. Br. 68.1: 36' x 32' R.C.P. Br. 67.9: 36' x 31' C. CULV. (Marlin) Br. 67.5: 24' x 26' C. CULV. (Marlin) Br. 67.1: 10' x 6' x 37' R.C. BX. Stock Yard Waiting Pen. TULSA CO. WASHINGTON CO. Br. 66.3: 95'0.0 RT. (C. Footings)

HUNDLEY SPUR

Sub. Red Xing. Pr. Xing. Pub. Red Xing. Pr. Xing. Pub. Red Xing. Sub. Red Xing.

Sig. 702 Sig. 601 Sig. 672

604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

618.9 604.3 604.3 653.3 654.3 657.3 649.3 638.3 640.3 640.3 638.3 623.3 623.3 626.3

66

65

64

63

62

61

→ To Tulsa

To Cherokee →

E1.2"
Sp. 160'E1.5"
Sp. 180'

Automatic Block

E1.1 1/2"
Sp. 360'

6300

627.3

0.0

0.4737

0.0

0.0

636.3

636.3

0.50

0.0

619.3

619.3

0.667

0.0

641.3

641.3

0.567

0.308

634.3

634.3

0.246

0.0

642.3

642.3

0.339

641.3

641.3

0.686

0.0

637.3

637.3

0.200

0.700

0.667

642.3

642.3

0.339

0.4726

0.4957

0.600

627.3

627.3

0.692

0.50

682.3

682.3

0.0

Pr. Xing.

Rub. Red Xing.

Rub. Xing. High St.
Sig. 652.

Pr. Xing.

Rub. Red Xing.

Saunders Cr.

Pr. Xing.

Hwy. 62.5
To Bartlesville
Pr. Xing.

Pr. Xing.

Rub. Red Xing.

Pr. Xing.

Rub. Red Xing.

Pr. Xing.

Rub. Red Xing.

Br. 6.5 x 20 x 24 C. G. G. (Martin)
Sig. 651.

Br. 6.5 x 20 x 24 x 27 T. B. X.

Shelter Ho.
Br. 6.4 x 30 x 120 R. C. P.

Pr. Xing.

Cont. Sig. 642

Br. 4.140 x 140 x 140 O. D. R. A. F. T.
C. Footings.VERA
M. 64.03

Br. 6.3 x 12 x 7 x 27 R. C. B. X.

Br. 6.3 x 10 x 7 x 36 O. D. B. on
C. Ab. B. D.

Br. 6.3 x 20 x 6 x 27 R. C. B. X.

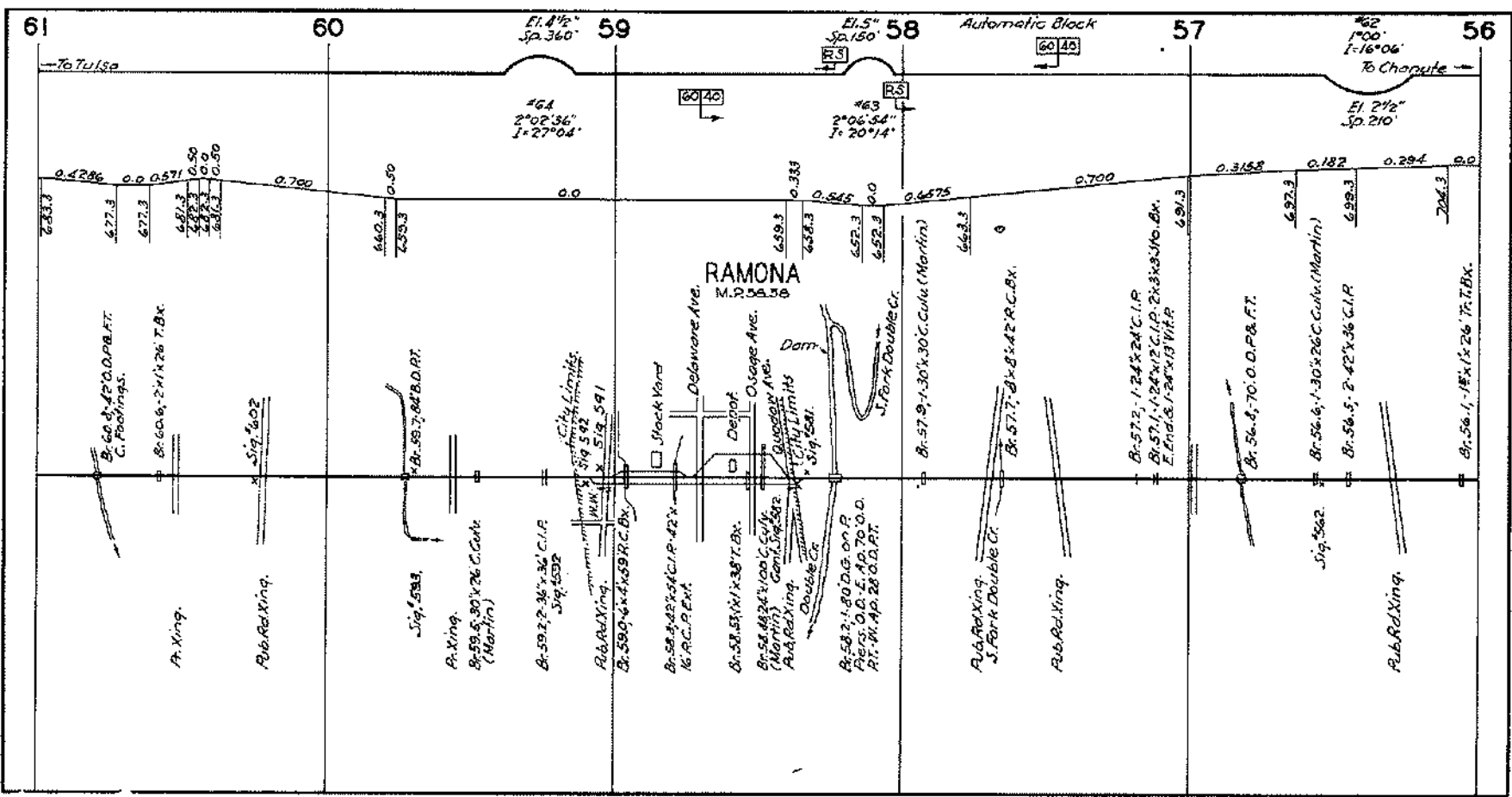
Br. 6.2 x 70 x 20 O. D. R. A. F. T.
C. Footings.

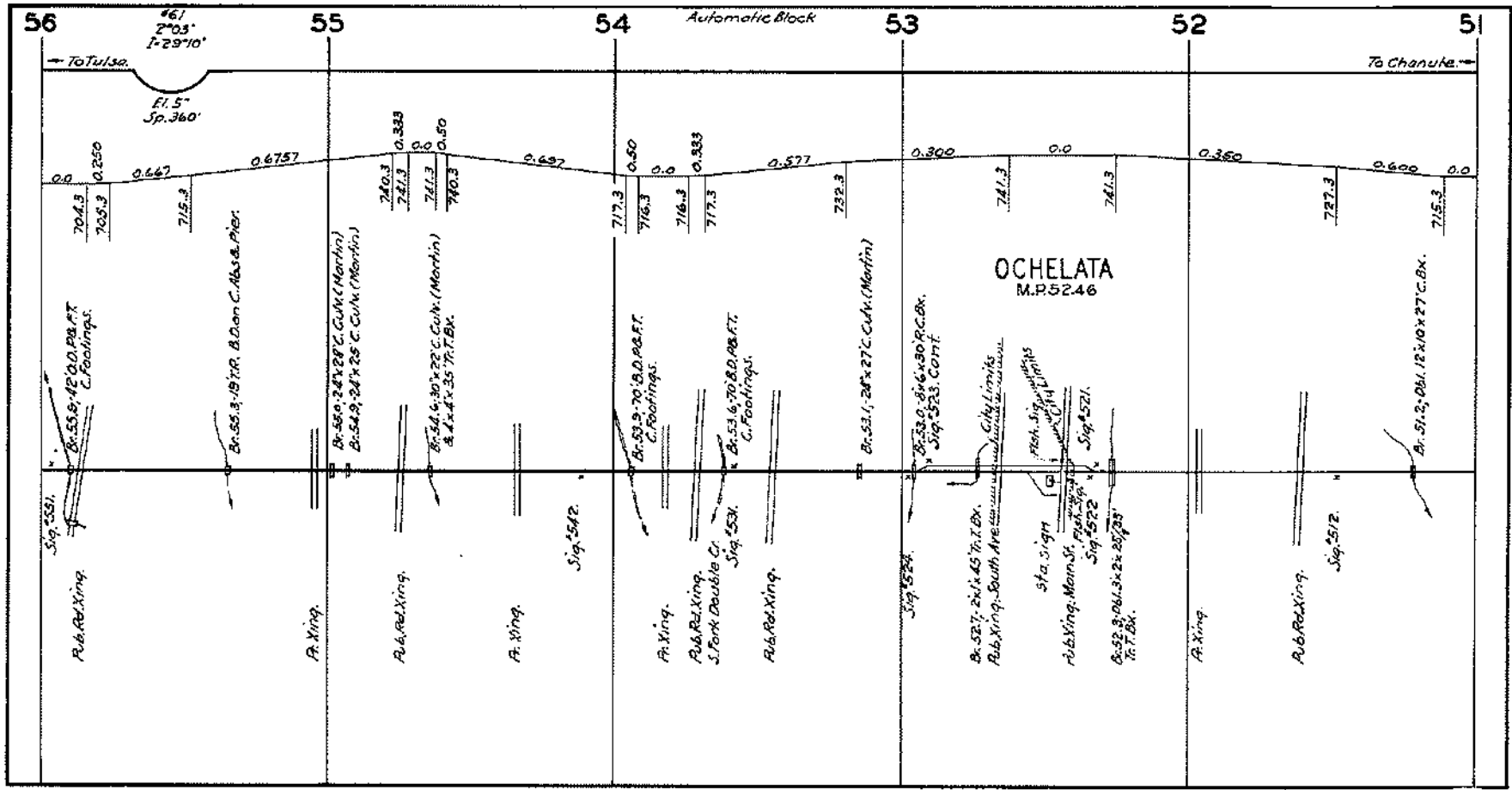
Sig. 622 x Br. 6.2 x 6 x 5 x 28 R. C. B. X.

Sig. 611. Br. 6.2 x 8 x 4 x 40 R. C. B. X.

Br. 6.1 x 126 x 0. D. R. T.

Br. 6.1 x 20 x 15 x 24 T. T. B. X.





56

55

54

Automatic Block

53

52

51

N 61° 2' 03"
P = 29° 10'

To Tulsa

To Chanute

El. 5'
Sp. 360'

704.3
705.3

0.250
0.667

0.6757

740.3
741.3
741.3
740.3

0.333
0.0
0.50

0.657

717.3
716.3
716.3
717.3

0.50
0.0
0.583

0.577

732.3

0.300

741.3

0.0

741.3

0.350

0.600

715.3

Sig. 531.
Br. 53.9, 42' x 0.0' P.C. FT.
C. Footings.

Br. 55.3, 19' x R. B. Don C. Als. & Pier.

Br. 54.9, 24' x 27' C. C. W. (Martin)
Br. 54.9, 24' x 25' C. C. W. (Martin)

Br. 54.6, 30' x 22' C. C. W. (Martin)
& 4' x 35' T. T. Bx.

Sig. 542.
Br. 53.9, 70' B. D. P. B. FT.
C. Footings.

Br. 53.6, 70' B. D. P. B. FT.
& C. Footings.
S. Fork Double Cr.
Sig. 531.

Br. 53.1, 20' x 27' C. C. W. (Martin)

Sig. 524.
Br. 53.0, 8' x 30' R. C. Bx.
Sig. 523, Cont.

Br. 527, 2' x 45' T. T. Bx.
Pub. Xing. South Ave. ~~to~~ ~~the~~ ~~main~~ ~~line~~ ~~of~~ ~~the~~ ~~city~~ ~~limits~~
City Limits
Sta. Sign
Fish. Sign
Pub. Xing. Main St. Fish. Sign
Sig. 522
Br. 52, 8' x 61.3' x 2' x 25' T. T. Bx.
Sig. 521.

OCHELATA
M.P. 52.46

Br. 51.2, 0' x 12' x 10' x 27' C. Bx.

Pub. Rd. Xing.

A. Xing.

Pub. Rd. Xing.

A. Xing.

A. Xing.

Pub. Rd. Xing.

Pub. Rd. Xing.

Sig. 524.

Br. 527, 2' x 45' T. T. Bx.

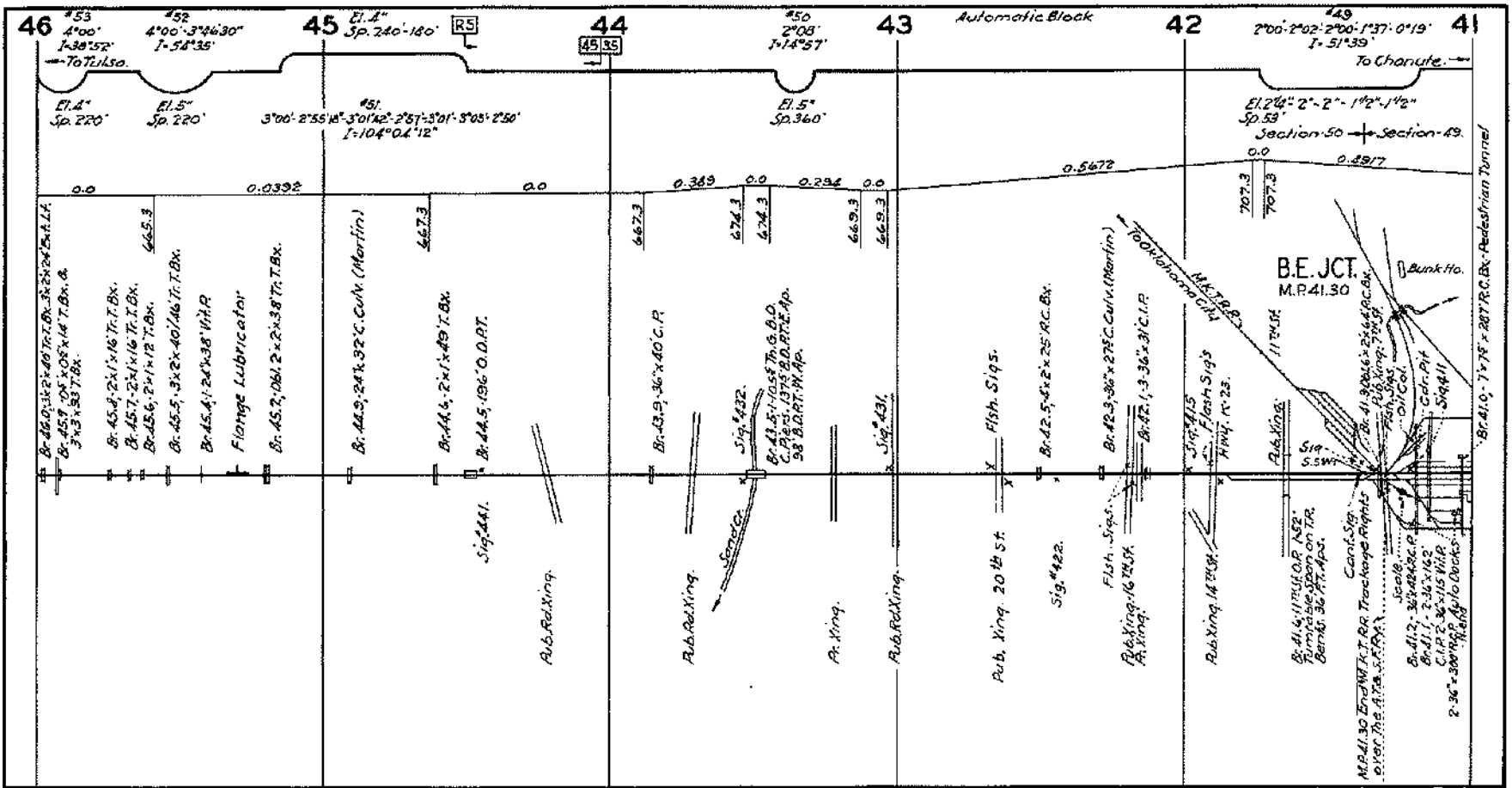
Pub. Xing.

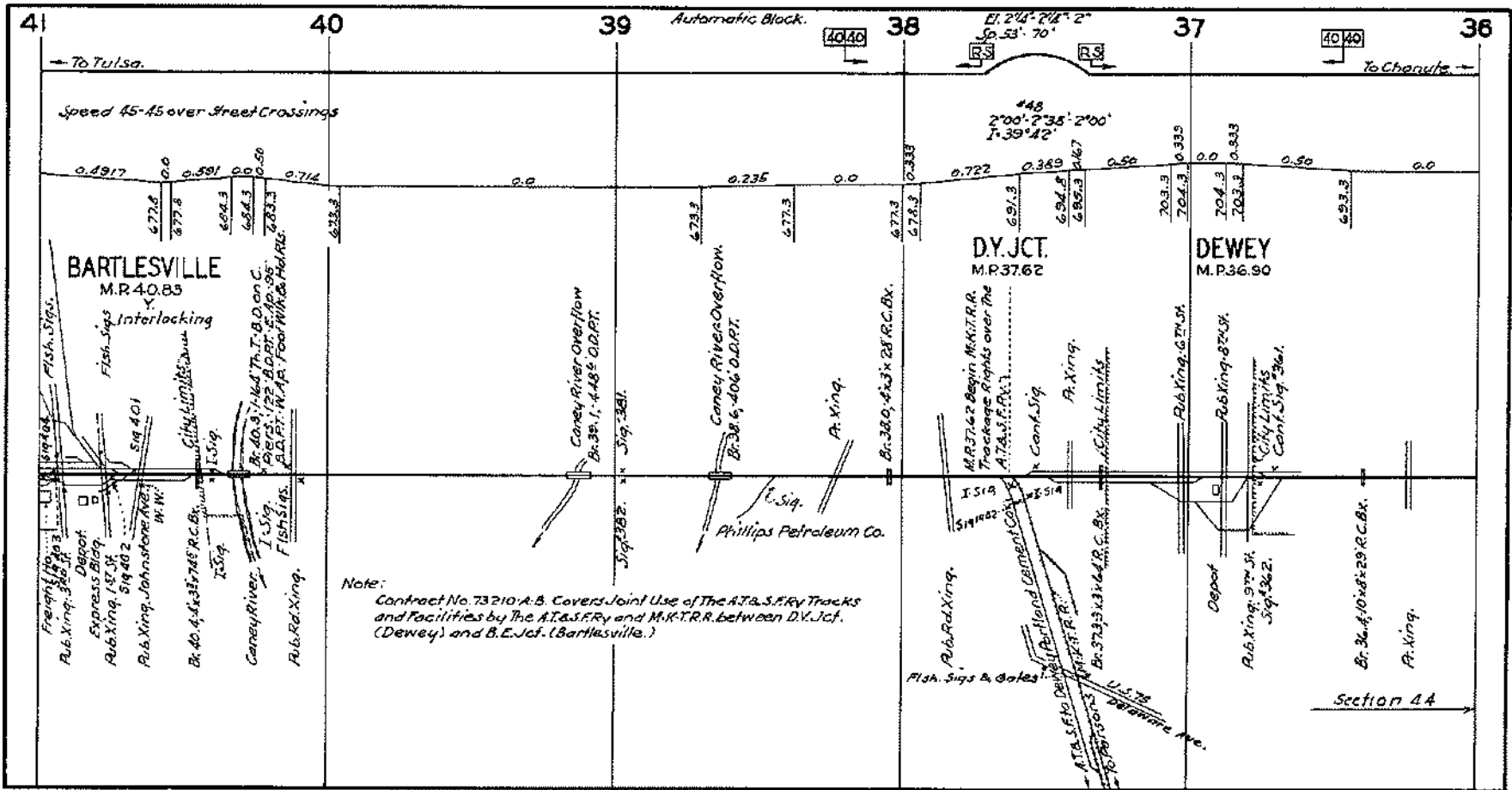
A. Xing.

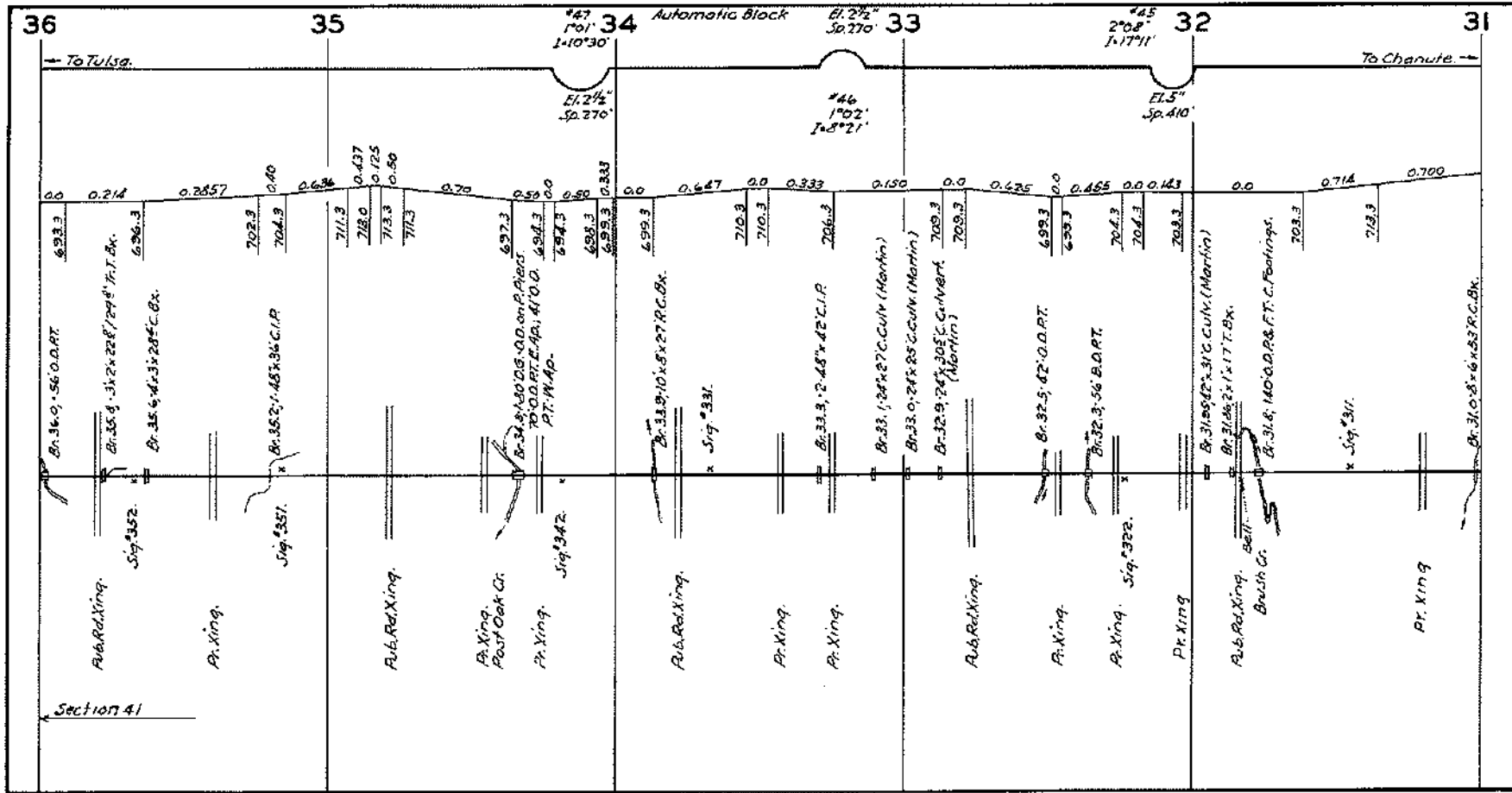
Pub. Rd. Xing.

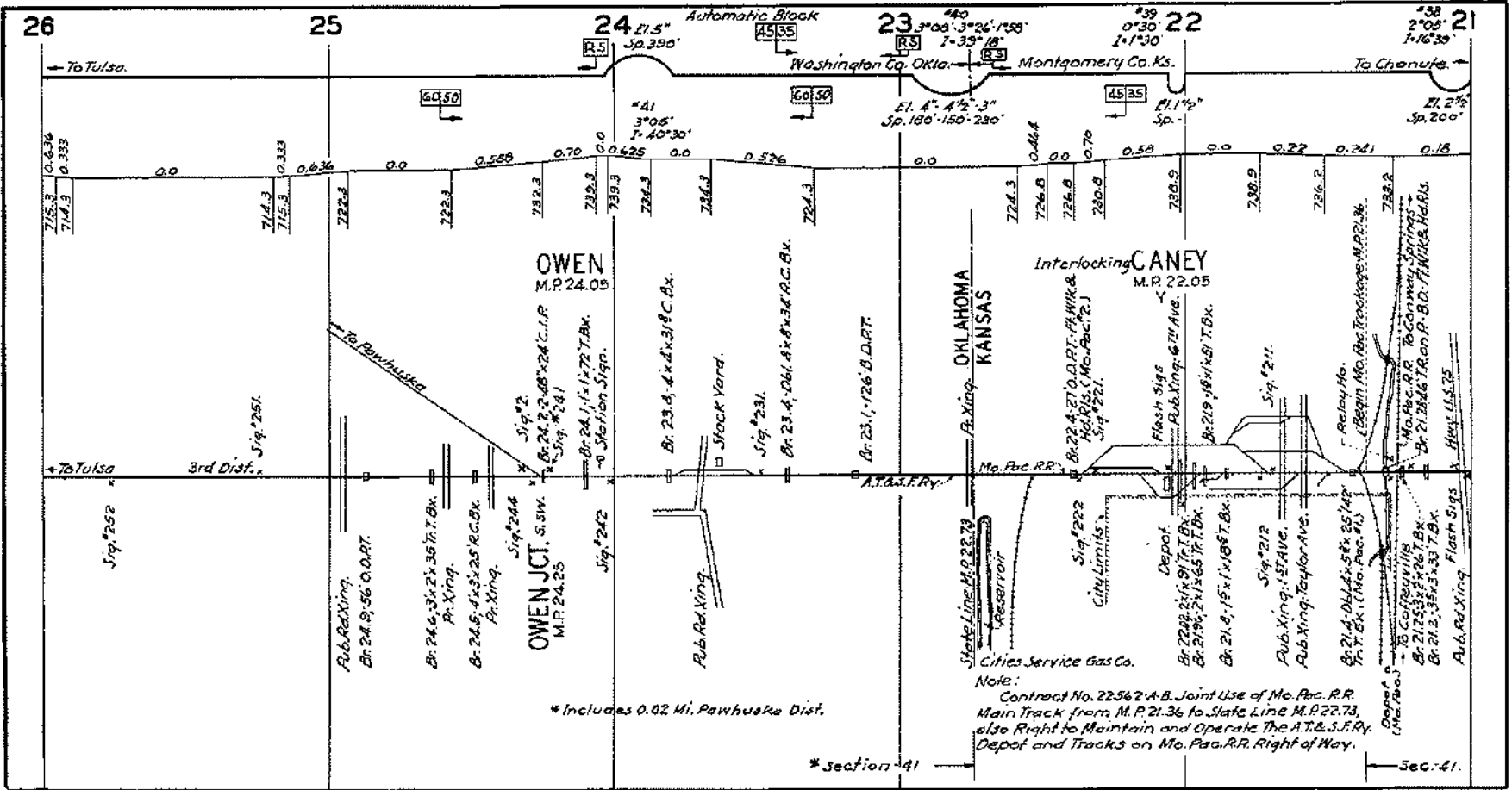
Sig. 512.

Br. 51.2, 0' x 12' x 10' x 27' C. Bx.









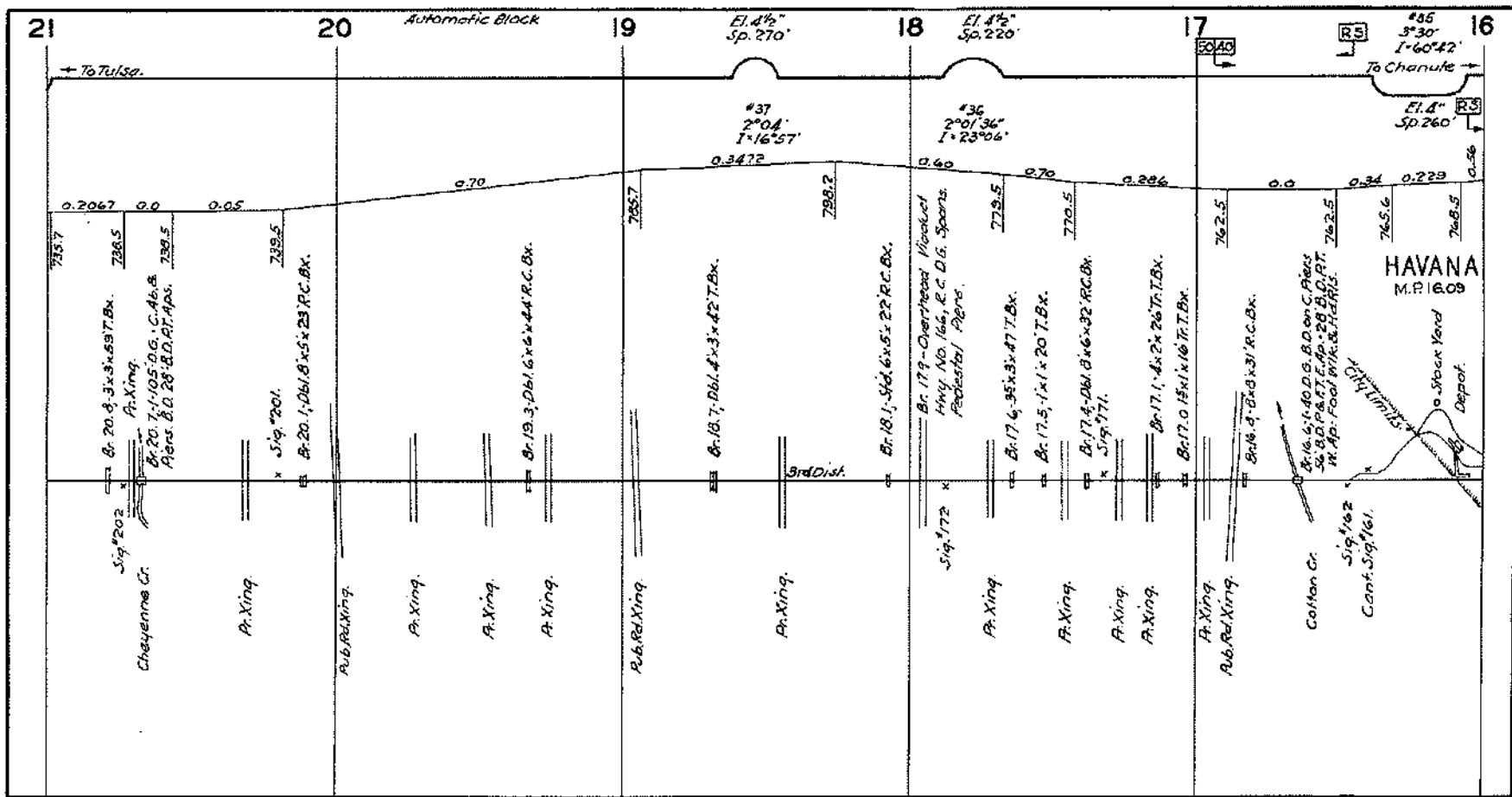
* Includes 0.02 Mi. Pawhuska Dist.

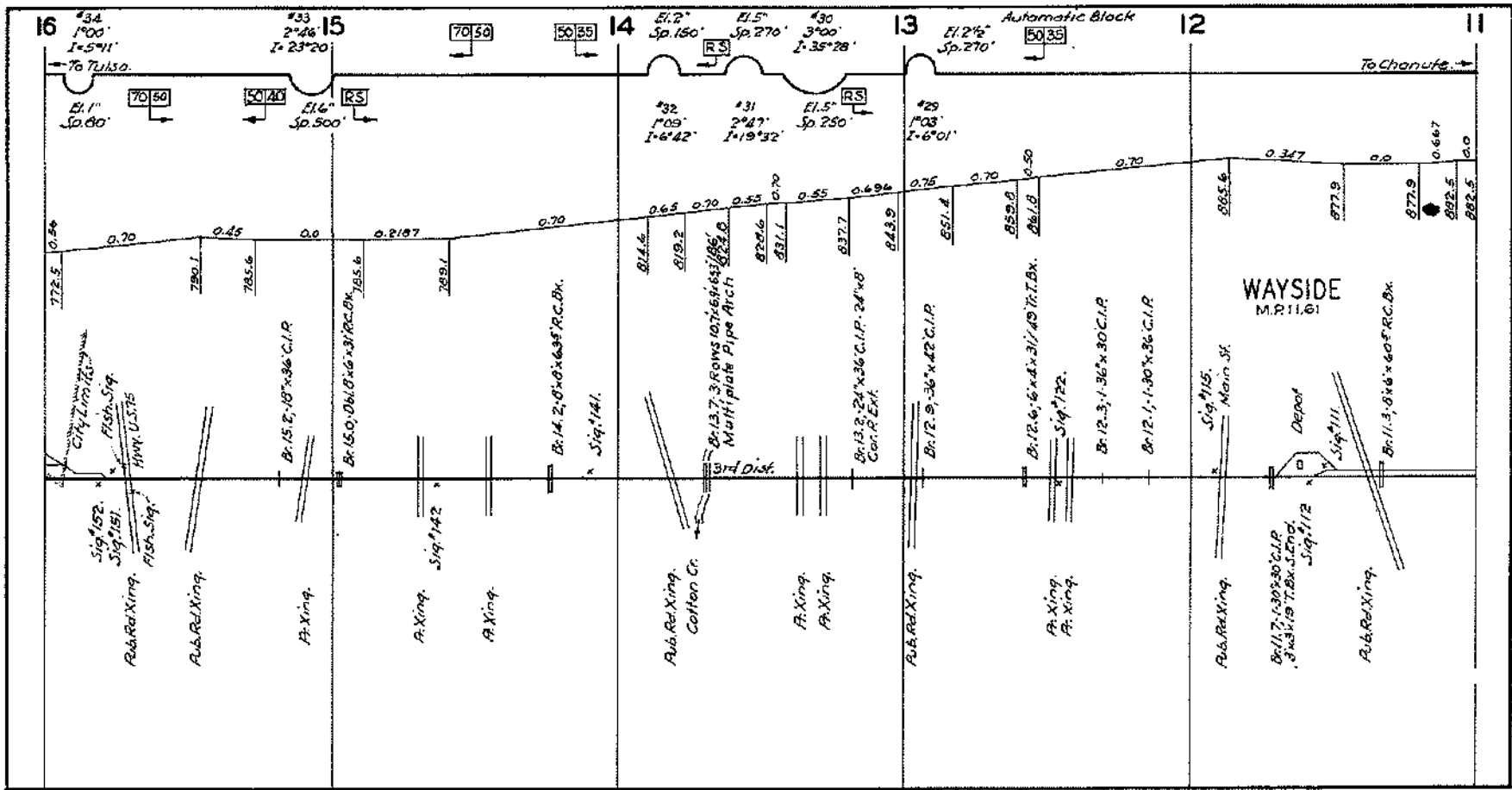
Note:

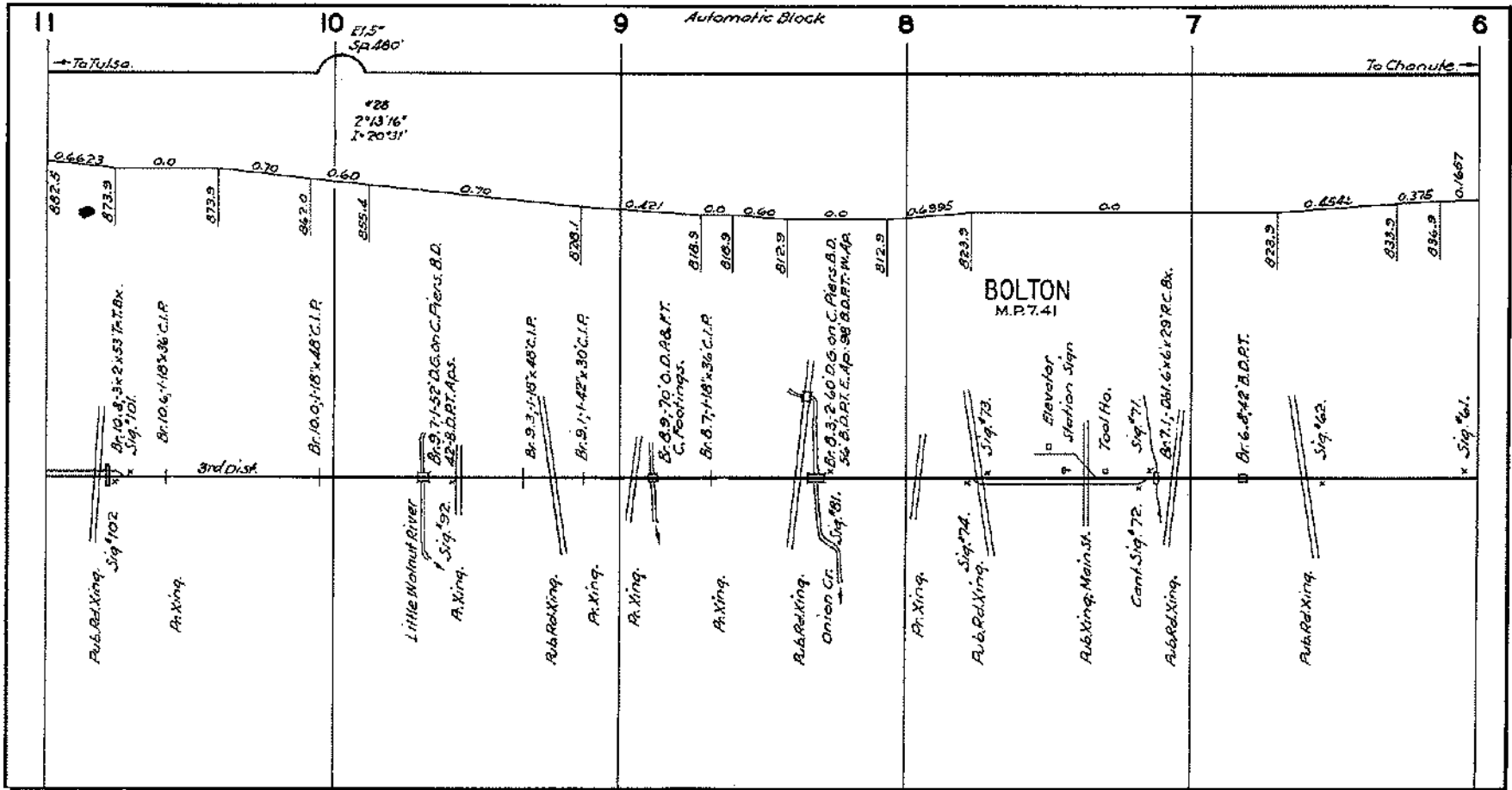
Contract No. 22562-A-B. Joint Use of Mo. Pac. RR. Main Track from M. P. 21.36 to State Line M. P. 22.73, also Right to Maintain and Operate The A.T.&S.F. Ry. Depot and Tracks on Mo. Pac. RR. Right of Way.

* Section 41

Sec. 41.







Automatic Block.

